

CB

AN ARGUS SPECIALIST PUBLICATION

CITIZENS' BAND

FOR BRITAIN'S 250,000 CB USERS

THE TEAM SPIRIT
Latest CEPT set



CAUGHT IN COURT
A cautionary tale

COMMUNITY RADIO
Why so long?

Latest equipment, clubs and QSL news

The THUNDERPOLE RANGE

Out of this World!

THUNDERPOLES ARE GO!

The Ever Popular Mk II (Base Loaded)

Base loaded ribbed coil design on a single radiating element 1.5m long. Doubly encapsulated windings for maximum performance and all weather protection.

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Bigger diameter, radiating element 1.65m long, making the **most** of the latest H.O. specifications.

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New Thunderflash Mobile (Centre Loaded)

Centre loaded helically wound solid copper winding on fibre glass shrink wrapped for maximum protection — strength.

The whip is 17.7 p.h. stainless steel and all fittings are chrome brass with stainless steel screws.

Slim elegant coil. Overall length of the Thunderflash 1.65 long.

Fully compatible for use on **27FM and new CEPT frequencies**.

To obtain maximum performance insist on:

THUNDERPOLE HATCHBACK/BOOT MOUNT

No drilling or soldering necessary. Standard 1/4" UNF. Thread mounting adjustable angle with positive non-slip action.

THUNDERPOLE STRIKES AGAIN! Citizens Band magazine on the fantastic Thunderpole range.

Thunderpole II

The Thunderpole was the best all round base station antenna tested.

A well constructed easily assembled and erected excellent performance legal antenna with the added bonus that it is pre-tuned.

Thunderpole III

On test the Thunderpole Mk III performed very well and contact was maintained even when the mobile test vehicle encountered a strong signal on the next channel in our notorious blackspot. In conclusion we consider the Thunderpole III to be a strong well constructed, top performance antenna.

Thunderflash Mobile

Firstly it is a very slim and neat looking aerial. The whip is 17.7 p.h. Swedish stainless steel and the centre loaded copper coil is wound on fibre glass for added strength and rigidity.

The screws themselves are stainless steel and all fittings are chromed brass. A very elegant antenna indeed. Reported TX was excellent.

It is strong and very well constructed and excellent value for money at around £16.00.

THUNDERPOLE ANTENNA U.K. LTD.

A subsidiary of Freeman & Pardoe Ltd.,

Tything Road,

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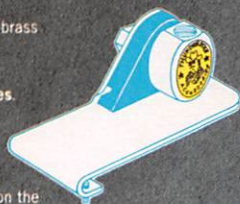
Ask for Thunderpole at your local stockist

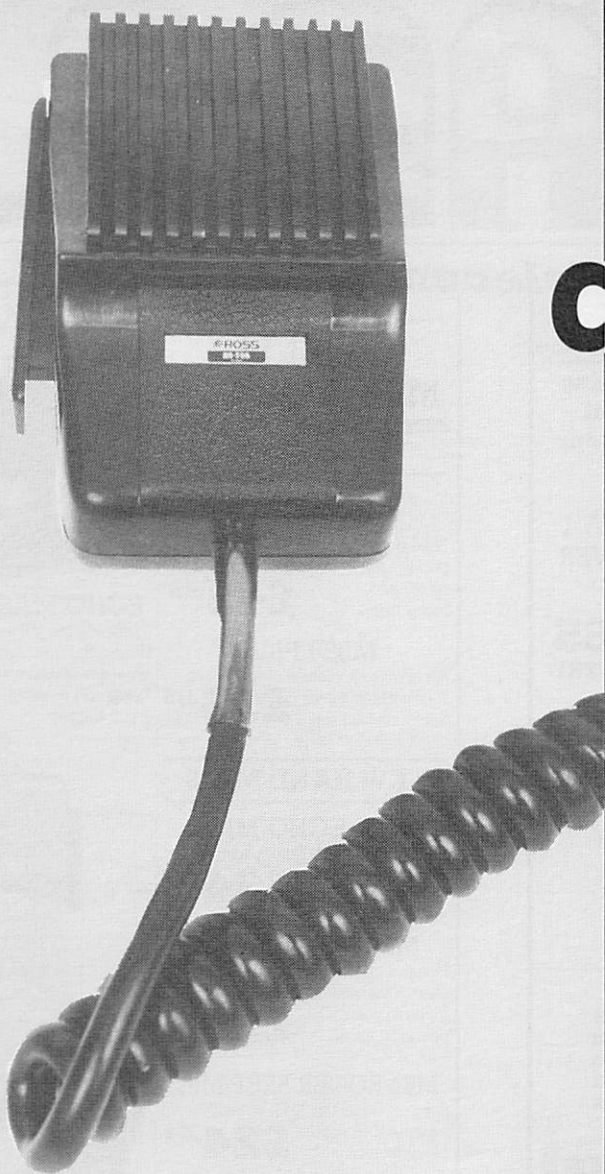
The Thunderpole design and name

are registered trade marks of

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CB

CITIZENS' BAND

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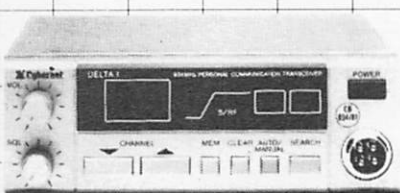
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934 Mhz PERSONAL RADIO EQUIPMENT

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CYBERNET DELTA 1 934 Mhz TRANSCEIVER

The UK's number one set with a sensitive receiver and many added features.

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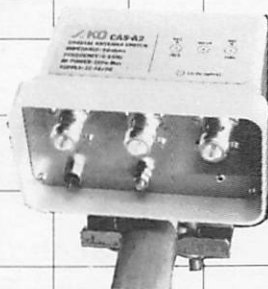
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£49.95

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PA7-E Base Colinear 7dBi Antenna	£67.75
TC12L 12 Element Beam 18dBi Antenna	£45
HRA 934L in line Pre-amp	£139.95
HRA 900 Masthead Pre-amp	£147.80



HL910R Mast Pre-amp/Pwr-amp	£199
WR 900 SWR/PWR Meter	£54.72
HP 5900 Deluxe SWR/PWR Meter	£89.95
WELZ 2 Way N Switch	£42.32
UHF 1200 25W Amp (For Lossy Feeder Runs)	£99.00

CB RADIOS

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- ★ AM/FM/SSB/CW
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BRAVO PLUS

Base power mic with crisp and punchy audio including modulation meter and roger beep.

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Puts the famous Sadelta echo onto any mic of your choice.

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K40 DYNAMIC MICROPHONE

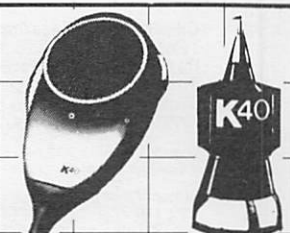
Housed in a rugged case with extra heavy duty cable and magnetic back clip.

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K40 MOBILE ANTENNA

Probably the best selling CB antenna in the world.

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K40 SUPER
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THIS MONTH'S NEW PRODUCTS



**MOBILE CHARGER
BCA4**

Allows the Charging from your car of any 10 nicad CB hand held with HI/LOW rate switch.

£17.95

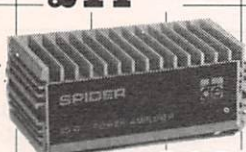


**UNIVERSAL
MAINS
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A mains charger that will charge from 1 to 10 AA size nicads at a constant rate.

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**FD30 30MHZ
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Operates from 13.8 Volt DC



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**SPECTRUM 200
5/8 Base Ant.**

A top of the range antenna suitable for 26 - 30 Mhz and powers of up to 2.5 KW Gain:- 6.8 dB

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**SPITFIRE
3 element Beam**

One of Europe's most popular beams, this antenna is lightweight and high gain. Freq - 26-30 Mhz. Power - 2 KW

£51

SALIUT 27

3/4 wave ultra-high gain base antenna. Adjustable 26-30 Mhz power max. 2 KW gain 6.5 dB. The ultimate base antenna for long distance work. Height 9.1mtrs

£73

NEW SPACELAB

Wideband 5/8 Base Antenna. 5.3 dB Gain. 1.5 KW.

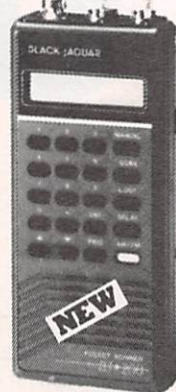
£64.50

THE UK SCANNER SPECIALIST Just arrived Black Jaguar MkIII

With additional features

Switchable between AM/FM
Base & Mobile Facility
Frequency range -
26-30Mhz, 60-88Mhz,
115-178Mhz, 210-260Mhz,
410-520Mhz

£235



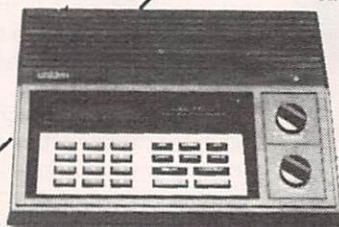
**100 XL
Bearcat**

Hand held scanner with 16 channels.

Memory scan covering -
66-88 Mhz, 118-174 Mhz,
405-512 Mhz.

Complete with carrying case earphone and charger unit

£189.99



Bearcat 175 XL BASE

Base station scanning Rx with 16 channel memory scan covering: 66-88Mhz, 118-174 Mhz, 405-512 Mhz.

NEW LOW PRICE

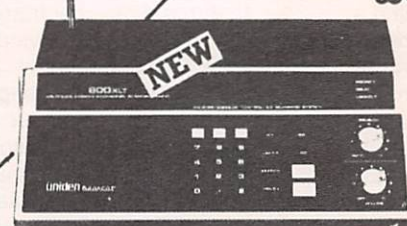
£179.99

**50 XL
Bearcat**

A super NEW low cost handheld scanner with 10 memories and covers -

66-88Mhz, 136-174Mhz,
406-512Mhz

£99.99



Bearcat 800 XLT

40 Channel Base Scanner
Covers: 29-54Mhz, 118-174Mhz,
406-512Mhz, 806-912Mhz.
Complete with AC adaptor

£257

**NEW 580 XLT
Bearcat**



100 Memory channels covers:
29-54 Mhz, 118-174, 410-512 Mhz.

£225 Requires 12V DC supply

**70 XLT
Bearcat**

Pocket size scanner with 20 memory scan covering 66-88 Mhz, 118-174 Mhz, 406-512 Mhz
Complete with carrying case, earphone and charger unit

£179.99



**NEW
BUMPER
CATALOGUE
£2**
Includes
£2 Voucher

NEVADA COMMUNICATIONS

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UPDATE



Editorial

OK, the good news is that the Department of Trade and Industry are arranging a meeting between themselves and various user groups sometime next month. Naturally, we aim to be there to ask some pertinent questions and try to get answers to the sort of questions you would like to ask.

Although the Department of Trade and Industry has come in for some flak of late – too long-term decision-making,

poor policing of CB offenders – to be fair, they are, in fact, listening a lot more to the user and meetings like this are happening on a more regular basis.

It seems that a lesson has been learned and they now realise they can't 'hide their heads in the sand' anymore as they have been accused of doing in the past. They now appear more willing to meet more and more with the people who actually use CB on a day-to-day basis. This can only be good news for breakers everywhere. Naturally, we shall publish any information and answers that come our way from this discourse – and, obviously, comment upon them.

Elsewhere in the magazine, we hope you enjoy our regular features plus some one-off specials that may interest you. In particular, we look at the problems involved in *complete* harmonisation. It still seems, at the moment, to be a case of the left hand not knowing what the right hand is doing. We shall see . . .

Camman Brival

New Swindon Club

A new club has been formed in Swindon – and anybody is welcome. The Swindon Citizens' Band United Club meet every Friday (from 7pm onwards) at the Jacobs Ladder pub, Ermin Street, Swindon, Wiltshire.

Anyone interested should contact Captain Styles, 32 Vicarage Road, Swindon (0793-20692), Yorkshire Rose, 88 Frobisher Drive, Swindon (0793-47981) or Crazy Legs, 65 Coronation Road, Wroughton, Swindon (0793-813644).

Aces High

The Four Aces Breakers Club will be holding their fifth annual Eyeball on Sunday July 17th at the Lee Memorial Park, York Road, Church Gresley, near Swadlincote, Derbyshire.

The open-air event will start at 11am and continue until 6pm, during which there will be a marching band, tug-of-war contest, donkey derby,

dog club display, fancy dress competitions plus beer tent and food and drink.

There is ample space for cars and buses – all free of charge. Trade stalls will cost £10 (£5 deposit) and charity stalls £5. Interested parties should contact Black Gold, Secretary, PO Box 6, Swadlincote, Burton-on Trent, Staffs DE11 0AA.



Special November Kilo Fete

Another event in Lincolnshire this year is an eyeball/fete, organised by the November Kilo DX Group. It takes place on Saturday 23rd July at the Boston Road Recreation Ground, Sleaford.

The eyeball will double as a fete, and will include local charity organisations, local businesses and traders.

The Group will draw the winning tickets on the day and prizes look good, including a colour portable TV set. The day starts at 10.30am and finishes at 7pm. Admission is free and any profits will go to various charities.

The November Kilo DX Group can be contacted at PO Box 4, Sleaford, Lincolnshire.

Cirkit Exclusive

Cirkit Distribution, which has marketed the Larsholt range of tuner modules in the UK for several years, has now been appointed sole and exclusive UK distributor for the range.

An acknowledged leader in the Danish electronics industry, Larsholt has developed and manufactured equipment for the technical industrial market for over 60 years. Stocks of its popular types 7254, 7255 and 8319 units are now held at Cirkit's Broxbourne headquarters and the range will be increased over the coming months.

The Type 7254 is a high quality tuner unit for the reception of FM broadcasts in the VHF band, the design gives a good combination of sensitivity and processing of strong signals at an economical price. Operating over the frequency range of 87.5-108MHz, the unit has a sensitivity of 1.2uV and total harmonic distortion of lower than one per cent. It is available in both stereo and mono versions. The mono version is particularly suitable for sound distribution systems in hotels, hospitals etc.

Type 7255 is a sophisticated stereo tuner for quality reception of FM broadcasts and features MOS-FET front end, low noise IF and high quality filtering. A number of outputs facilitate a wide range of functions, making it especially suitable for use in synthesised tuners.

Type 8319 is a front end unit for use in high quality FM receivers. Its MOS-FET RF and mixer stages along with a high stability oscillator make it particularly suitable for the first stage processing of FM broadcasts. As well as excellent sensitivity for weak signals, the unit gives very fine processing of powerful signals, claims Larsholt.

Summer Catalogue

The summer 1988 edition of the Cirkit constructors' catalogue is now available,



and features many new products for the electronic constructor in its 184 pages.

Being shown for the first time among the 3,000 lines are a low cost 10MHz oscilloscope, the Easiwire fast circuit prototyping system, an automatic NiCad charger-cycler, two new models of hand-held scanning receivers, a 2-5,000MHz RF power meter, and the 'Rolls Royce' of digital multimeters – the Avo M2000 series.

The new products are complemented by additions to Cirkit's components range, with enlarged selection of NiCad batteries, mains connectors, Scart connectors, DC fans, RFI filters, high current chokes, ABS and 19-inch rack cases, rocker switches, tip cleaners and cyno adhesive.

A special attraction for the first 500 constructors to send in an order over £500 (excluding VAT) is a free digital travel alarm with date, hours, minutes and seconds display. Cirkit is also running its popular competition once again, with one of the new 10MHz oscilloscopes as first prize. Prizes for runners-up are auto-ranging multimeters and electronic toolkits.

For customers wishing to place orders by telephone, Cirkit's system has been improved by making (0992) 444111 a direct line through to the sales desk. A new number (0992) 441306, is now available for enquiries to other departments.

For further information contact: Cirkit Distribution Ltd, Park Lane, Broxbourne, Hertfordshire, EN10 7NQ. Tel: (0992) 444111.

Hopscotch Mass Eyeball

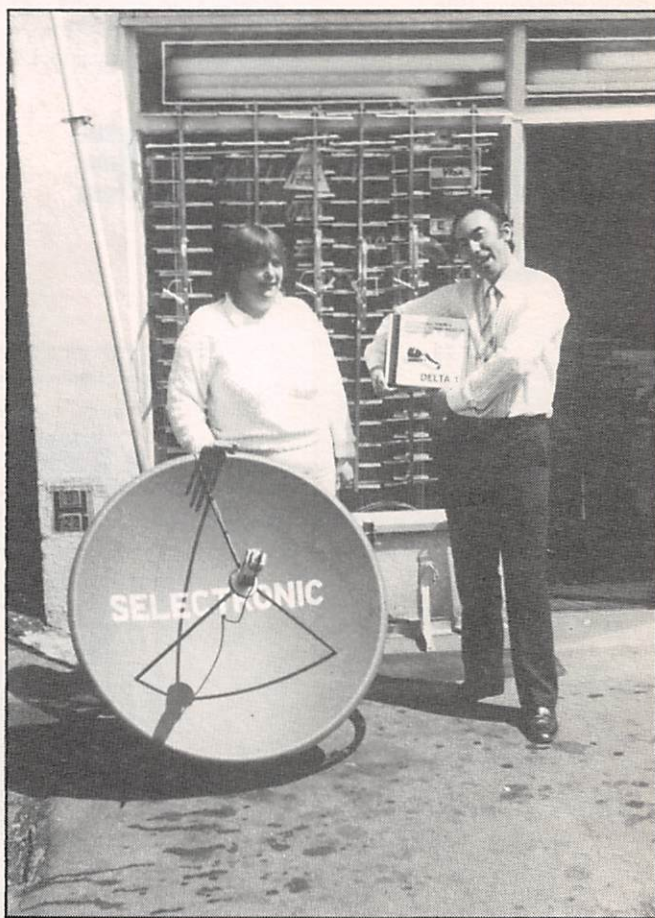
After the success of their October '87 Eyeball, the Hopscotch CB and DX Club of Skegness recently announced the date and venue of this year's event.

It will be held at the Richmond Hotel car park, Skegness, and starts at 12 noon. Doors open at 11.30am

and entertainments include licensed bars, fast food, disco and (thankfully) live music – and children are welcome.

It should be a 'goodie' as last year's event raised £500 for the Children's Special Care Unit, at Pilgrim Hospital, Boston, Lincs.

The Club can be contacted at PO Box 25, Skegness, Lincolnshire PE24 4LU.



Don't Fret – Not Yet!

The recent statement from the Department of Trade and Industry regarding the future of 934MHz prompted Nevada MD Mike Devereaux to pen this note to us:

"In the statement, the DTI have now confirmed that, from December this year, we may no longer import or manufacture transceivers for this band. However, many users have mistaken this to mean that we will lose this band from that date. This, of

course, is not the case, and the DTI have been at pains to state that the band may be used for the foreseeable future – and certainly for the lifetime of their equipment.

"On a brighter note, Phil Jeffery, our UK Sales Manager, took this photo of Mike and Mary Machin whilst he was delivering an order of 25 Delta Ones to Selectronics, who are obviously going great guns on 934MHz."

Children in Need 1988

Last year the Merseyside Breakers (better known as the Mike Lima's) raised £954 for the 1987 Children in Need Appeal by spending 48 hours part-way up one of the mountains of the Horseshoe Pass in North Wales, asking fellow CB enthusiasts to donate money.

This year, they intend to pull all the stops out and go for a 75-hour modulation from 1pm Wednesday 16th November to 4pm Sunday 20th November.

Permission is being sought

to use the 'Pudsey Bear' logo on a special four-part A4 QSL card, which will form part of a package return on all donations received; club and personal cards, certificate of contact and a few other things will also be in the return package.

A special feature of this year's event will be a children's section, complete with Father Christmas on the mike to talk to the kids. A special 'kids' pack will go out to all donations.

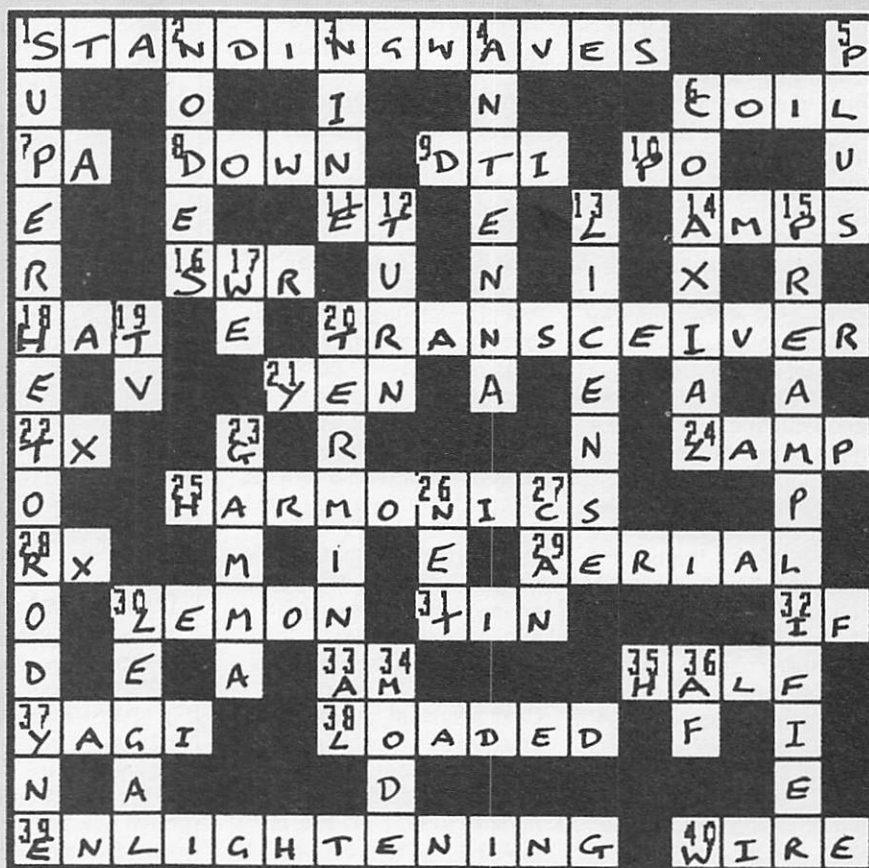
Merseyside Breakers can be contacted via PO Box 24, Birkenhead, Merseyside, L42 9GE.

MERSEYDEE BREAKERS



Crossword Answers

How did you get on with June's CB crossword puzzle? Check out how you did with the answers here.



Pama & Co

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uniden

100



One of the most popular Mobile Rigs around. It has excellent sensitivity and anti 'bleed over' facility of the 200.

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200



One of the best mobiles around with R.F. gain, tone control and other features, it has a good sensitive RX section with low 'bleed over'.

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300



Undoubtedly the 300 is the best selling base station in the U.K. It's features includes mic gain, R.F. gain, power and tone controls and S meter. Value for money? It's unbeatable.

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The first available rig for CEPT frequencies manufactured to the highest standards. Features include mic gain, R.F. gain, squelch, 40 channels and a low 'bleed over'.

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PAMA ARE PLEASED TO ANNOUNCE THAT THEY ARE NOW APPOINTED MAIN DISTRIBUTORS OF THE MUCH ACCLAIMED RANGE OF VALOR ANTENNAS.

NEW THIS MONTH

uniden PC 450

This rig is so new that at the time of writing this ad, we don't have total information. By the time you read it we will have delivery of stocks, so why not phone us.

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MIDLAND - **REVADA**

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THUNDERPOLE - SIRIO

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NEW

Pama SUPA TRI

EXCEPTIONAL PERFORMANCE FROM
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INTRODUCTORY
OFFER
PRICE

Retail Price

£19.95

**GET £5 OFF THE SUPA TRI AERIAL
WHEN YOU PURCHASE THE
UNIDEN 400**

ALSO AVAILABLE

The original **PAMA TRI** our No. 1 best selling aerial is still available made from the highest quality materials, the PAMA TRI has a performance unequalled by any antennae in its price range.

ONLY £14.95

MIDLAND 805

AVAILABLE
FROM
MID JUNE



Exceptional performance from this dual-purpose hand portable. Can be mounted under the dash and when away from the car - hiking, walking, fishing etc. - it will keep you in touch.

MIDLAND 104

AVAILABLE
MID JUNE
MIDLAND
MINI-
MOBILE



It doesn't need to be big! The Mini Mobile is the smallest rig available but don't be deceived; it has exceptional performance for its size.

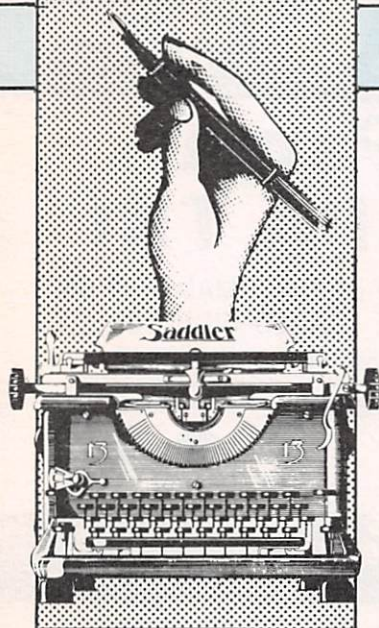
**PAMA HOUSE
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3 MINS FROM M56**

OPEN 7 DAYS OF THE WEEK

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Pama & Co



More Responsibility

I have just read "Back Chat" for the first time and must disagree with Martin Wright from High Wycombe.

Obviously, he has a major grouse with the DTI who, admittedly, do not appear to be over friendly towards CB. But I ask myself - is it all their fault?

The serious sidebanders as such, are usually a knowledgeable bunch but I'm afraid a lot of those using 27MHz FM have little or no interest in radio. Some of them couldn't give a "monkey's" about the effect of twiddling or the use of illegal power amplification, often with what only could be described as 'poxy' audio.

It is a small wonder that the authorities have a jaundiced view of CB when you hear repeated foul-mouthed obscenities from young breakers, many of whom have got a 'full setup' for £20 or so and seem oblivious to the fact that a licence is required.

As Martin says, CB is the seed that sows the interest and a large number of newer radio amateurs began life on 27MHz having realised that (a) your average CB black box is only mediocre at best; (b) usage of frequencies from 1.8MHz to 1296MHz gives much more satisfaction; (c) one can experiment with aerials to one's heart's content and

(d), perhaps more important, that the knowledge available from CB is somewhat limited.

Having said that radio hobbyists belong in the amateur radio service, Martin can hardly complain that the DTI's intent (his personal view) is for CB to remain an ineffective and useless hobbyist's toy.

Perhaps if some Cbers were more responsible, showed a little interest in radio mechanics and consideration for channels other than the ones being used (are you listening, Jock?) then the wonderful hobby of radio could be just that.

On the brighter side, as from January next year, radio licences are to be amended to grant facilities on a reciprocal basis with countries who adopt the Nice 1985 suggestions.

Whilst it is early days yet, and Martin will, no doubt, be jaundiced, won't it be wonderful to be able to go abroad with one's gear!

Don't be such a sourpuss, Martin. Take your ticket and have the best of both worlds.

Geo Broadhurst (G1F6A)
Accrington



Freeway in Despair

I am a new CB breaker, my handle is Freeway, and my 20 is Leeds 13 in West Yorkshire. I have been using my CB rig for about four weeks now and have discovered CB to be a very interesting and rewarding hobby.

I have become known to a few local breakers who are, for the most part, sensible and very helpful towards me and I have a good ratchet with them now and again.

Today I became the proud owner of a brand new £12 CB licence and, as far as I know, from the information contained within it, I am fully legal.

However, the aforementioned is not what I want to talk about. What I want to mention is the irresponsible, ignorant, stupid morons who use the breaker channels (19 and 14 in my area) for foul language contests, lovers' quarrels and all sorts of loutish behaviour too numerous to mention here, instead of making contact and then getting the hell off the aforementioned channels.

Quite a number of the breakers who are guilty of this behaviour have been using CB for quite a number of years, yet they still persist in making life a misery for others, myself included.

I have read somewhere quite recently that the majority of CB users are

considerate and helpful towards newcomers and even go out of their way to show newcomers how to get the best use from CB.

Apart from the small circle of breakers I ratchet with in my area, I have yet to find any who are helpful, let alone considerate.

Don't, for goodness' sake, say it's only a mindless minority who do such things. I won't believe it. Anyway, at least I have poured my heart out to someone at last.

Here's hoping the s**t doesn't hit the fan.

PS. Does anyone want to buy my CB equipment - I have given up in despair.

**Freeway,
Leeds**

Thanks for the Rescue

If I may, I would like to use your column to thank a number of breakers who came to the rescue of my wife and family a couple of months ago.

About three-quarters of the way up the "Tak-ma-doon" road in Glasgow, the car's petrol pump gave up the ghost and there we were, stuck with not another car in sight.

However - no problem!

Within seconds of my Mayday call, Friday Girl and Meatloaf of Stonehouse, Wild Turkey of Airdrie, The Cruiser of Tollerness and Groundbait of Motherwell were debating how best to get us mobile again.

The AA were organised and, in just over an hour, we were heading back downhill and home on the advice of the AA. On the way back, I tried to contact our new-found friends and benefactors but, apart from getting out a brief "We're mobile!" as we started, I never got through to thank them properly.

I should mention that, from the moment I was advised that the AA were organised, there was hardly a time when there wasn't a car or motorcycle stopped to see if help was required. Chivalry on the air and on the road is still very much alive.

Many thanks, lads and lassies - I owe you!

**Double D,
Glasgow**

Protect Original FM

The day draws ever nearer when the so called "Review" of our CB channels takes place.

Already the conditioning process to accustom our minds to an inevitable loss have started. Constant referrals to the declining number of licences taken out, and the assertion that this will be a major factor in any decisions taken are the order of the day when any question to our "Masters" is made.

As an example, the first edition of the annual report of the Radiocommunication Division of the DTI gave CB barely a general mention, but the new edition gives a full-page graph of the declining take-out of CB licences.

While we accept that there has been a steady decline over recent years, we must also note that the graph has flattened out considerably since the beginning of 1986. We should also take cognisance of the fact that, in the earlier years, only three CB sets were covered by each licence, but now an unlimited number of rigs are allowed since the new licence came out, and we would suggest that the number of CB sets actually in use are nearer to 300,000 than the 121,696 licences suggest.

In this year's annual report, on page 16, the term "Usage" is used, and ABCB

consider this to be a very important factor indeed. Therefore, in conjunction with The Radio Club of Nottingham, a "Usage Comparison Analysis" form has been produced which will enable CB folk to do their own analysis on how many folk are on which channels on the old (UK) and the new CEPT channels. As a guide, just before this article was written, a random check showed 38 channels in normal use on the UK frequencies and nine on the CEPT. If this trend continues, then we consider we have a very strong argument indeed for the retention of our old (UK) frequencies.

The important factor will be to get these analysis forms out all over the country, and to this end, CBers are invited to send a stamped addressed envelope to ABCB, PO Box 13, North PDO Nottingham NG5 7DU.

Unless we put up a well organised fight, we shall lose our old (UK) frequencies and at the moment, these are the frequencies by far and away, that are most popular with the CB users.

**The Leftman,
Chairman ABCB,
Secretary, The Radio Club,
Nottingham**

Well Said, That Man

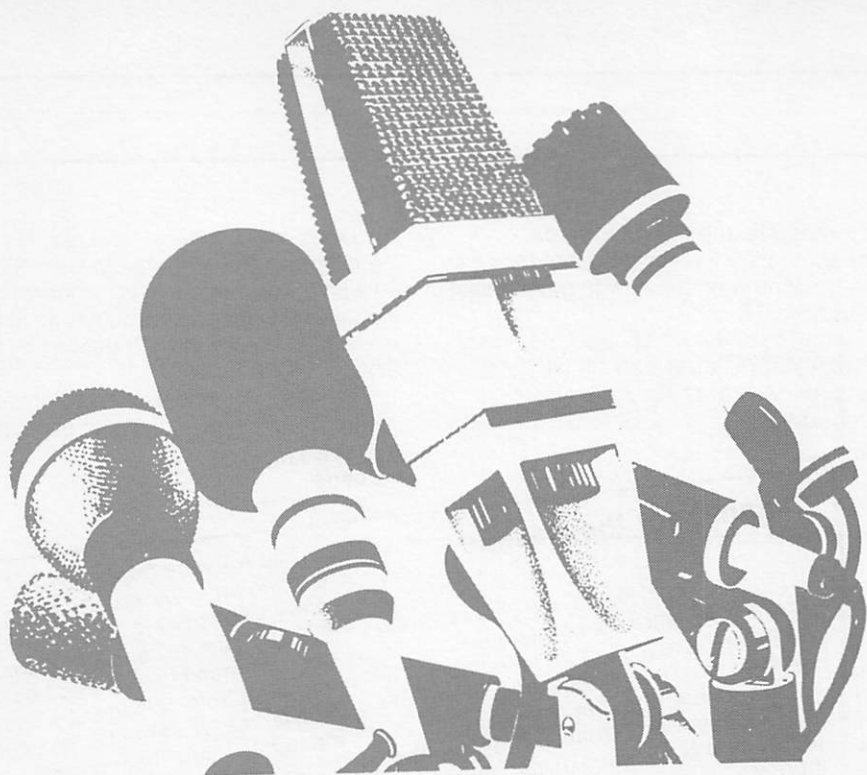
I was recently listening to a BBC Scotland radio magazine programme called *Head On*, when I heard the presenter say that he would be talking about citizens' band radio in a piece called "Whatever happened to?" Imagine my surprise when he introduced none other than Eamonn Percival, editor of CB mag!

I don't know how many other breakers heard the show, as it goes out quite early in the morning, but I am sure they were as excited as I was upon hearing our hobby, which most of the media probably think is dead, being discussed on the radio.

I'm not buttering the editor up just to get my name published but he actually did do a very good job in promoting the real cause of our hobby - community care and the rest of the good work done by groups raising money for charity.

Only one moan - I think the Editor should quit chain-smoking as I thought I was listening to Rod Stewart when he started to speak!

**R. Reid,
Auchenlerie**



OVER THE AIR

Shepherd Man is worried about the future of 934MHz, but also finds time to provide us with an update on CB clubs

Well, my very first pack of QSL and Eyeball cards have arrived from Des Currie. They arrived with a heavy thump on the floor. Which led to a fight between me and the dogs to see who was going to open up the package! Quick thinking on my part and, after saying the magic word, I won. Needless to say that the postman lost, as they decided to chase him down the road instead. Thank God my postman has a sense of humour!

Over the first May Bank Holiday, I visited Auchenlarie in Scotland and attended the Cree Valley Eyeball (a report to follow) and met many old friends. It was a great weekend full of the things you would expect from a Scottish Event. Thank you to the young lady who gave me her stockings as a memento – better than any eyeball card lads! She is the daughter of a very well known Scottish CB family and many of you will know her father! On the way back, I got stuck in the most horrific traffic jams on the M6 and M1 and I am sure that I would have still been there now if it wasn't for 'Ginger' from Stoke-on-Trent (a trucker) and 'Gold Top' from Luton. They helped to guide my old skateboard along the A508 and onto the A5 in an effort to miss the traffic jam on the M1. My thanks to both of them for the help – as well as the lighthearted banter. Hope to catch you both again soon.

I have no doubt that everyone will have heard about or read about the closedown of importation and manufacture of 934MHz equipment for the UK. And I am also sure that some of you will have your own views about this! Maybe many of you are even saying "So what? It only has about 3,000 users so

why should I care?" Well, I think that you *should* care! It has been known for some time now that 934MHz would have to share with others and from what we already know from our fellow CB users, they have always had problems with certain telephone and, up until now, they have managed to live with the situation. However, when the Government starts to bring in SRR (Short Range Radio) in 1990, how long will it be before the band's 3,000 users find it impossible to use their sets?

From the Press Notice (DTI 88/156) they make the following statement: "Once SRR becomes widely used, it is inevitable that the CB service will suffer an increasing level of interference and it is important that potential users of the band are aware of this, and that many potential users of the new SRR service are adequately protected".

The first thing that springs to mind from this statement is the word "potential". From the first time it is referred to I tend to think that they mean "potential new CB users" but who in their right mind is *now* going to go out and buy a 934MHz rig when it is clear that the usage will be even more limited in the future. The only way to get out on a set will be by throwing it out the window. From the second part of the above section of the statement, I take the word "potential" to mean "interested new users" i.e. companies or businesses. In which case, how come they can offer them protection when we have always been told that "CB is an unprotected service"? Perhaps what they really mean is: professional users are happy to pay more, and therefore deserve better protection.

Let's now consider another part of this Press Notice i.e. the following statement: "It is of course only fair that existing users of the CB band get a good life from their sets. I can assure users that their existing equipment may continue to be used for its foreseeable useful life". Well, it has already been stated that, they plan to open this band up to SRR in 1990 and that 934MHz CB users will encounter problems. So what is the "useful life" of the sets? Do they mean up until 1990? Or until present CB users get squeezed off of the band? As for Mr Butcher's assurances that "existing equipment may continue to be used", that is in effect like saying that the world won't end tomorrow! As I understand the term "useful life" it means "until the bloody thing won't work anymore". So, is he going to assure us that the present users will be able to use the band in exactly the same way as they do now – and cover the same distances etc?

The second paragraph of the Press Notice starts off: "The Government in common with other administrations in Europe is to consider the introduction of a Short Range Radio (SRR) system in the band 933-935MHz". Now, remember the use of the word "consider", and then

the third paragraph starts off: "Once SRR becomes widely used". So, are they just thinking about it or have they already decided on bringing it in? well, if they haven't already decided, why withdraw MPT 1321 as from 30th December 1988?

On the second page headed "Notes For Editors", it states that the RD has met with the two largest 934MHz user groups "the 934MHz UK Club and the Personal Radio Club of Great Britain. These groups have been fully consulted about the withdrawal of the specification". Did they know about this? Were they fully consulted? When did they know about all this?

By now, I hope that you will have started to take an interest in what I am trying to say. Because the best is yet to come . . .

We are always being informed that the future use of present CB within the UK (UK FM 27MHz 27/81) depends by and large on the amount of licences sold. Yes, this has been said by the DTI. And as we all know these numbers are falling (see DTI/RD Annual Report 85/86 & 86/87). These, we are now told, are down to 115,000 (Press Notice 88/156). Take into account a certain percentage loss through 934MHz and the downward trend before the 1990 Review, it is very possible that we may lose the use of this band. Never, I hear you say but consider the following:

1. That we have been given a new alternative frequency (band) - CEPT, and that this has brought us inline with other European countries.
2. That a common European frequency would allow us to take "a common CB set" abroad without customs problems.
3. That if we all move over to CEPT, 27MHz could, and would, be taken up for something else in the UK and abroad, thus possibly offering another useable European frequency.

They could never shift us, I hear you say! Couldn't they? They have offered us CEPT for Europe. With regards to UK FM 27MHz 27/81, they have more or less said "use it or lose it", even "pay up or shut up". Well, we are not paying for it (licence fees) and they *could* get a lot more money from 27/81 by selling it off to private/company use. They couldn't wipe us out, I hear you say! Couldn't they? Remember what they are doing to 934Hz and the effect it will have to the users.

Remember that ALL CB bands are *not protected* and as such, they can take it away as easy as they gave it. Or move us to another part of the Spectrum - CEPT!

We are all aware of the "official" user numbers quoted - and we know that there are still around 250,000 CB users within the UK (legal or otherwise). However, the Government require *cash* to function and that is "fact not fiction". We, as users, are also aware that the main reasons for the drop in licence

figures are down to the fact that many Cbers are sick of having to put up with "bucket mouths" and "muppets" - so they refuse to renew their licences. We know that a percentage of breakers will not even take out a licence because they stand very little or no chance of being caught by the RIS or police.

We also know that another percentage refuse to buy licences because they don't consider that "they get value for money". And so the list goes on . . . Then of course the RIS/DTI turn around and say "because we have not got the revenue - we can't do much about the problems". So where does it all end? Are we playing into the Government's hands by *not buying* licences - thus giving them the chance to move us all over to CEPT? They may not gain anything with regards to licence sales by moving us over but it will give them our old 27MHz band to sell off and make up on finances - even make a good profit. Well, if you're not interested in what may happen to CB after the 1990 Review, then you may as well start using your set as a door stop *now*. But for me, I don't much fancy the thought of sharing what could turn out to be our "one and only legal frequency" with a bunch of spaghetti-eaters who push up to 1KW of power.

Some months ago in *Citizens' Band* a breaker asked for some clarification of the listing in the DTI (RD) Annual Report for licences issued under the heading of "Police CB Monitoring". According to the Licencing Section at Waterloo Bridge House, these were issued some time ago and used by the police to track illegal CB stations. However, they are also used by them when searching for

"lost children" etc when working in conjunction with Cbers. At some point in the future these sets will come under the Police and Fire Licence - and will not require individual licences.

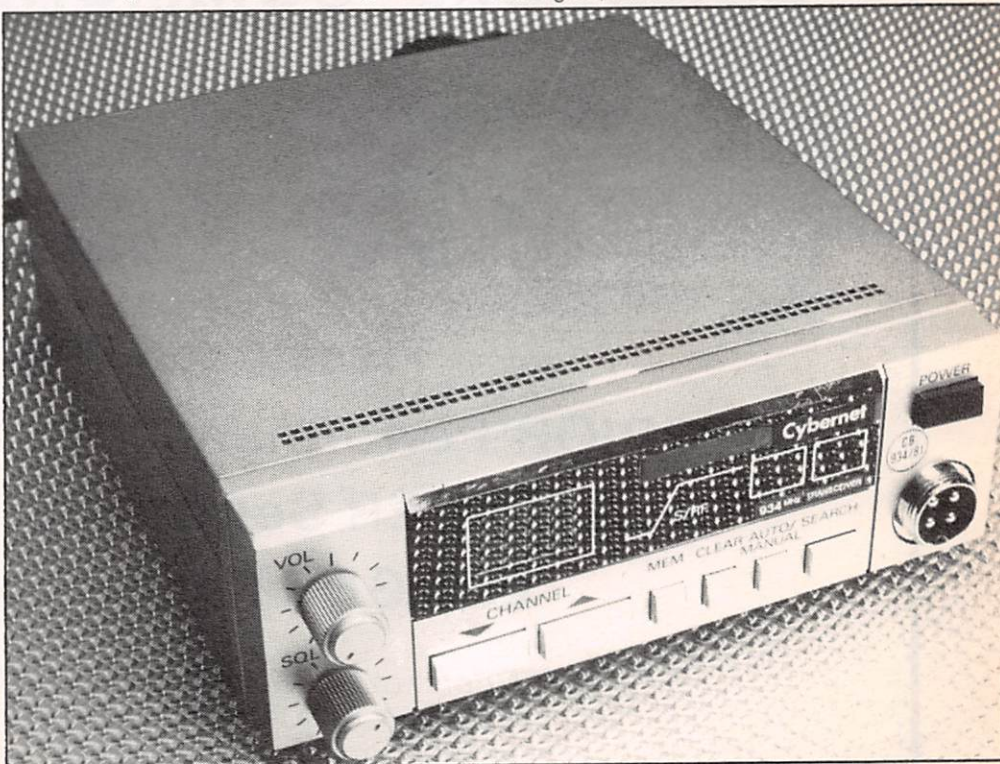
A number of breakers are showing concern as to "what is and is not" legal to use on the new CEPT frequency. In particular CB sets, so to clarify the point listed below are the current Type-Approved sets:

Zodiac Communications Ltd	M 144
Zodiac Communications Ltd	M 244
Zodiac Communications Ltd	PT 040
Uniden UK Ltd	PC 400
DNT Contact	40 FM
DNT Contact	Coupe
DNT Contact	Transit
DNT HT	4000 FM
Albrecht Electronics	AE 4002
Albrecht Electronics	Alpha 4000
Danituf Radio UK Ltd	Danita 285

It is advisable to check with the RD in London (01-215 2171) if you are unsure, as a large number of breakers are being sold sets as "Type Approved" when they are not! If this does or has happened to you, then contact your local Trading Standards Office, because you are covered under the 1968 Act. They will look into this matter on your behalf and help get your money back.

I now have an address for the new Kingston-based Kilo Tango Radio Club. It is PO Box 47d, New Malden, Surrey, KT3 3EU. They meet every other Thursday evening at Norbiton Workmen's Club, Elm Road, Kingston-upon-Thames, Surrey. For further information please contact The Secretary, who is Lady Mariner.

The Chairman of Natcolicibar, Brian Babington, has sent me some news of a



change of address for the CEPT Offices. These offices control the European Standards and from the 31.3.88 they can be contacted at: Institute European des Normes de Télécommunications, Sophia, Antipolis, Nice. He recently attended the ECBF Meeting in Athens and was given this information as well as the following, which are answers to questions asked by members of the European Parliament over the years: that CEPT recommendations T/R 20-02, T/R 20-04 and T/R 20-07 are considered as satisfactory and the CEPT intends to establish a European nominal for telecommunications covering CB. Also that the European nominal be used in the future work programmes for CEPT, and that it should begin from 1st January 1988, and that a Commission must take into account technical questions put forward during the "definition period" of the paragraph covering the whole subject of the European Norm for Telecommunications in relation to CB sets.

At the May ECBF Meeting in Athens, Peggy Tapper (Secretary of Natcolcibar) was voted by the members into the job of Senior Vice President to the ECBF. This will be a great boost for Peggy, as she is recovering from an operation and unable to attend the meeting.

Some very sad news in from Yogi Bear (Chairman of the Breaker-way CB Club), London SW19. The Club President Nelson (Golfer) passed away

one the 18th April aged 82. Nelson, who had been the President since March 1984 had always been active in his support for the club and a well liked breaker around his home 20. As his handle would suggest, he was, in his younger days, a "professional golfer" and his interest in the sport continued even into his later years. The club have decided to honour him by awarding the "Nelson Cup" to the winner of their club golf tournament, to be held every year. A tribute to the way in which he was liked and well respected was that around 100 people attended his funeral and many of these were his fellow breakers. The

Chairman would like to express his thanks to all those who attended.

Although it is not common practice for me to stop and eat at "truck stops" (because truckers tend to park on top of my skateboard), I must mention one very good one that has come to my attention and run by fellow CBers. You can find it at the lay-by just after you come off the M3 (Sunbury) (London-bound side). "The Mistress" (Martha) will offer you good service and very good prices. Call her up on Ch 19 UK FM or pull in for an eyeball between 7am-4pm Monday to Friday. Look out for the converted ambulance in the lay-by.

CB Events

July 31st. Warminster Eagles Eyeball at Longleat Safari Park. Contact the club at: PO Box 13, Warminster, Wilts BA12 4YZ.

August 13th. Romeo Hotel DX Club Mass Eyeball at the Footballers Arms, Pilkington Road, Radcliffe, Manchester. From 10am-5pm. With Country & Western Music in the evening. Contact: Romeo Hotel, PO Box 2, Radcliffe, M26 9DB.

August 13th & 14th. Trail Blazers Eyeball at the Old Vic Hotel, Wolverhampton. Open all day and music on Saturday Evening. Plus the European POMA Collectors Club will be there again this year. Contact: PO Box 201, Wolverhampton, WV10 9HH.

August 20th. Tripe City Breakers Eyeball at the Barton Equestrian Centre, Nr Preston. For more details contact: PO Box 14, Accrington, Lancs. BB5 6JL.

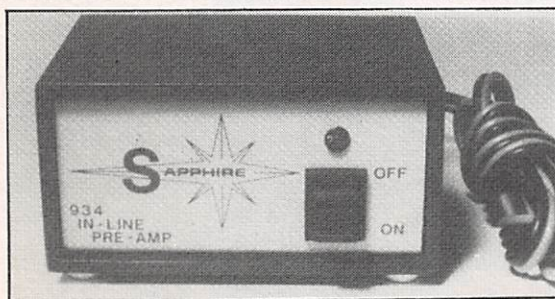
September 10th & 11th. Sierra Tango's 6th Annual Eyeball at the Robin Hood Camp, Coast Road, Rhyl, N. Wales. Further details from: PO Box 8, Rhyl, N. Wales.

October 7th, 8th & 9th. Solway Pirates Mass Eyeball at the Southernness Holiday Village. Plenty to do during the day and live groups every evening. Further details from: PO Box 15, Kirkcudbright, Scotland, DG6 4DH.

October 9th. Rolling Hills DX Club "Skegness Big eyeball" from noon until 6pm. More information from: Bottle Man, PO Box 17, Spilsby, Lincs. PE23 5JE.

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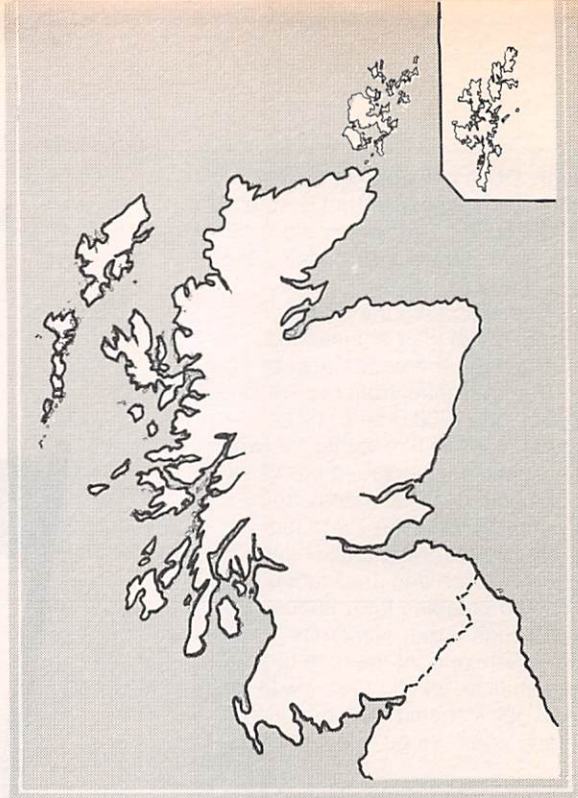
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The Scots Connection

Bunny looks at the latest on the Scottish scene

The holidays have been and gone. I did manage to get away for a few days, here and there with my son, Brian. Managed a couple of days in Northern Ireland, and eyeballed some friends there. Let me introduce you to CARES - Community Advice and Radio Emergency Service. It has been in operation since April 1982, and is affiliated with ALC (Active Line Communications), which was founded by a CARES member, with support of other people. The two groups run in conjunction with Channel 9 and the mobile Channel 19. Both are monitored on CB and over 7,800 calls have been attended to since April 1982.

Neighbourhood Watch Service has become an integral part of the service, and has been a success in the Ayrshire district of Scotland. They have received 99% co-operation from the law enforcement services and, in return, have given 100% effort to assist the recognised 999 service ashore, and to the VHF Channel 16 service afloat, because they are an aid to these services and *not* a substitute. If you would like to know more about the work CARES and ALC carry out, please write to: Mr. C. Carter, 14 Charles Street, Kilmarnock, Ayrshire. Please enclose an SASE.

In April 1988, Clyde Coast Search held a sponsored modulation at West Kilbride. The event began at 6am on 2nd

April and finished at 6pm on the 3rd. During this time, 1,100 stations called in from Dundee, Edinburgh, Bathgate and Northern Ireland to name but a few. The money raised will be donated to the RNLI. The club recently bought a caravan for the club's use, and hope to attend several eyeballs to spread the word about Channel 9 monitoring. Interested? Then contact Blue Top at PO Box 17, Greenock, Renfrewshire.

Another club that has been fund-raising is the Lima Foxtrot Group of Lockgilphead, Argyll, who also held a sponsored modulation from 9.30am on 16th April to 3pm on Sunday, a total of 30 hours. Donations will be used to purchase tapes for talking books for the blind, and a loop system for the hard of hearing in the local church. Other charities which will benefit are the local hospital and Cancer Research. Over the 30 hours, 504 copies were recorded but the bugbear of sponsored modulations appeared - skip - and blotted out several copies.

A request from the club; has any CBer got a spare tape recorder or spare tapes that they can send, to enable more tapes to be recorded for the blind. Please address to: Long John, LF Group, PO Box 2, Lochgilphead, Argyll.

Another satisfied customer comes in the shape of the Solway Pirate CB QSL Club in Kirkcudbright. They changed printers to Des Currie: "He has reduced the size of our membership certificate and has printed new invites (which has

already produced 39 new members this year. He has also produced 8-colour embroidered patches which are a joy to behold. The next bastion to storm is the rubber stamp and the cards, which will finalise a set of superior printing and stationery."

Two full CB setups purchased last year by the Solway Pirates have been put to good use in the houses of housebound breakers. One has already returned the loaned setup, and has bought her own. Her handle is The Duchess and her ZYL is Red Rover. She is 69 years young, and is plaguing Tartan Lad to raise her antenna weekly till it reaches Mars.

I attended the Mass Eyeball at Auchenlarie on the last weekend in April and took a trade stand in conjunction with MSGB. We managed to sell car stickers, log book souvenir news sheets, CB mags, and gave away FTI leaflets, Monitor and SCB news sheets, and joint cards. The weather remained sunny and warm, but there were many bleary eyes first thing in the morning. May I say "Thank you" to the organisers of the Eyeball, as we all enjoyed it and will, hopefully, return next year.

News of a new club that has opened in Prestwick, in Ayrshire. Named the Kyle and Carrick CB Club, it meets in the Auchenoyle Hotel (near the railway station) every Thursday from 7.30pm. Everybody will be made most welcome.

A request comes from the Armadale Pedal Pushing Squadron near

Edinburgh. Does any breaker have a 2-channel CB transceiver that is surplus to their needs? If so, please contact Brian, 8 Lower Bathville, Armadale, West Lothian EH48 2JR.

News comes in from the Solway Pirate Breakers of their annual Mass Eyeball at the Southerness Caravan Park, in Dumfriesshire, from the 7th to the 9th October 1988. There will be 6-berth caravans to hire, either for two or three nights. Also included are three nights of music to suit all tastes from music with a Scottish flavour to the "jungle" music that we usually hear. Further information and application forms can be obtained from Sunray (Brian), 83 High Street, Kirkcudbright DG6 4JW, Scotland. At the time of writing, I shall be joining forces with the "Monitor" news sheet from London in a trade stand again. Hope to eyeball you there.

A new record has been claimed by the non-UK club of the 2nd City Breakers in Glasgow. They have received a completed membership form claiming the farthest away member in in a non-UK club. Can your club beat that? The application was from a Dr. Tetsure Horie (handle Charlie). He was also interested in CB shops and various

literature about CB radio in Scotland.

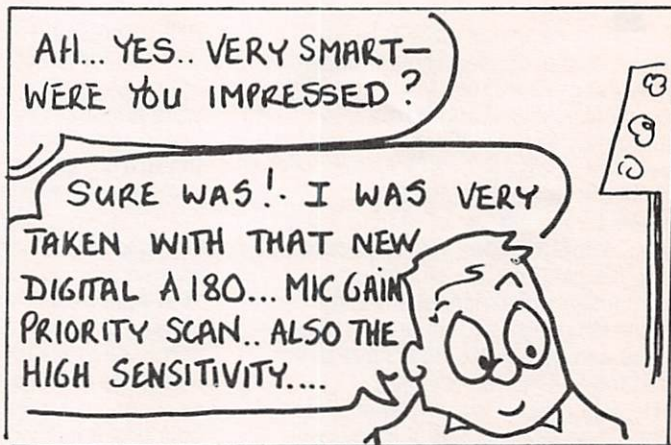
That's about it folks for this time; keep the articles rolling in. Please remember that I write for the mag three months

ahead of the publication date! Send all correspondence to: Scottish Connection, PO Box 337, Neilston, Glasgow G67 3JP.



WIRES ©1988.

by Mouse



COMMUNICATION THE

QSL WAY



More names and addresses from QSLers all over the world, courtesy of David Shepherdson

This year I'm happy to say that I've had a lot of clubs getting in touch with information on their various eyeballs and other such events. So many in fact, that I've had to restrict the details in the Forthcoming Events Slot to simply showing the date, place and contact address with a few details on some of the more imminent ones to start off with in the column itself! I remember that as little as two years ago, I was hard pushed to be able to give you information on as many as 10 events in a year; now I sometimes have more than that in one month! If you send any info please send it to my home address, 3 Tarn Villas, Cowpasture Road, Ilkley, West Yorkshire, LS29 8RH and should you wish a reply, please remember a self addressed stamped envelope (SASE).

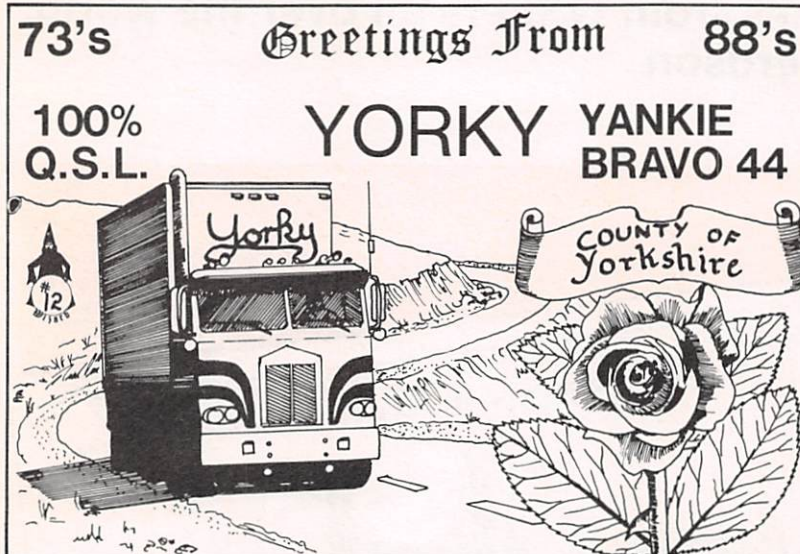
I have this very week received information on two eyeballs in July, the first is the Alpha Victor Group of Wales who are holding an Eyeball on the 30th but, unfortunately, apart from a contact address, that's all the details I've got! The second is held on the 31st by the Warminster Eagles at Longleat Safari Park from 10am until 6pm and admission is 50p which also allows you access to the grounds. The eyeball will be held in the grounds, to the left of the lake and there are various fun events throughout the day with club and trade stalls.

Moving on to August there's five events with one having a change of date and venue from the information I gave earlier this year. Apologies for any mixup there but I was going from information received. The first is on the 6th August



at Bristol, again only a contact address received. On the 13th there's a Mass Eyeball held by the Romeo Hotel DX Club at the Footballers' Arms, Pilkington Road, Radcliffe, Manchester from 10am until 5pm with a C&W Evening from 8pm until 11.30pm. Evening tickets £1.20, the day's Eyeball is free.

The same weekend there's the Annual Trail Blazers Eyeball and POMA Card Swap Meeting held at the Old Vic Hotel, Wolverhampton, admission 50p per day. Times are 10am until 4pm Saturday and 10am through to 2pm Sunday with entertainment on Saturday night supplied by a disco and featuring a



QSL Club Addresses:

Blue Star	PO Box 1, Cramlington, Northumberland, NE23 9LD, England.
County Colour Card	PO Box 106, Canterbury, Kent CT1 3YN.
Currie Card/Bank Clubs	PO Box 5, Consett, Co. Durham, DH8 8NG.
Firebirds	54 Boyne Road, Hastings, East Sussex TN35 5NZ.
Formula One	PO Box 14, Brighouse, West Yorkshire HD6 2SE.
Friend Link	Kevin, 25 Bruce Street, Barnoldswick, Colne, Lancashire BB8 5EU.
Kendal and District	PO Box 37, Kendal, Cumbria LA9 6RJ.
Ken's Kommandos	PO Box 93, Oldham, Lancs.
Sierra Charlie	PO Box 27, Southport, Lancs.
Tango Papa	PO Box 13, Southport, Lancs.
Trailblazers	PO Box 201, Wolverhampton, WV10 9HH.

When writing to any QSL Club of Firm, always include return postage to assist with their reply; it does help.

Country & Western Evening (admission £1) Many of the European POMA Collectors and Clubs will be attending as usual.

The change of date and venue one now, which is the Tripe City Breakers 2nd Eyeball held on the 20th at the Barton Village Hall, (not the Equestrian Centre) which is about two miles from Garstang on the A6. Times are 10am to 5pm with an evening disco from 7.30 until 10.30. Admission by ticket costing £1 plus SASE.

The last one in August is also on the 20th and is the MGX 32 Group Eyeball but once again I have no other details bar date and contact address. Moving on again into early September, there's a possibility that the ORC of Derbyshire may be holding a repeat of last year's Grand Social Evening but I haven't heard from them as yet. On the 11th September there's the "MAD DXers at Lansdown Race Course". My apologies here too, as I was given incomplete info on the Club and thought it was the MAD DX Group of Aldershot but I have now been informed it is in fact the MAD DXers of Bristol! Sorry for any inconvenience caused to either club or anyone writing to one club and getting a bemused reply!

Then there's the Sierra Tango DX Club's Eyeball held at the Robin Hood Camp, Rhyl over the long weekend of 9th, 10th and 11th. Chalets may still be available from around £12 per night. If interested in any of the above, please drop the club in question a line with SASE and request "Eyeball Info". If you could be bothered to mention that you saw the info in these pages I'd be obliged and it could help the organisers too!

Just a few lines here now for the organisers of any events, either later this year, or for those planning towards next year's meetings. First of all, I do need plenty of notice as I write these pages well in advance and the more time you can give me means the better chance of getting your details in more than one issue! Also, because of the success of the Events slot, I also include a listing of all the events of which I am aware into each Dragonrider Club package. At the time of writing, this holds details on over 30 eyeballs. Obviously by the time you read this, many of these will have been held, but I'll be adding a forecast for 1989 to it soon! In addition to plenty of notice, some details of where the event is being held along with times and admission charges (if any) would be most welcome!

Whilst talking about eyeballs in general, it's great to see that there is still so much interest in QSLing and not just simply in sending out cards, but getting yourselves out and putting faces to the names. No matter how I try, and I'm sure you've done it too, I always build up a mental picture of someone, perhaps

from their cards, or the way they write a letter, then when we meet I'm amazed at just how far off I've been!

Having said all that, I was at one such eyeball earlier this year where I was asked if I could ask you, the readers, for help. In the past I've asked for your help in finding a club, or in some cases, a QSLer whose address had been misplaced, but this time I'm asking for your help in locating a printing plate! Barry (Yorky) had a very early Wizard Series card done to his design which Barry is very pleased with and now, as he is running very low on these cards, wishes to have a reprint done. Unfortunately, the Wizard has ceased printing cards through ill health some years back. To the best of my knowledge John (JR) of the ABC Club in Cheshire took over the Wizard series but Barry has been in touch with John who has said he does not have the plate Barry wants. If anyone can help Barry in his search for the plate, please get in touch with him at 2 Chestnut Grove, Outwood, Radcliffe, Manchester. Thanks in advance for any help you can give to him!

As I promised last month, I'd like to start to include some names of the many, many people who I managed to meet at the various eyeballs I managed to get to this year. These begin with Bill and Angela (*Steam Pusher* and *Chocolate Raisin*'), Allen (*The Exploited*), Steven (*Powerslave*), all of the Consett area. Brian (*Scot's Cavalier*) of London, John (*Green Falcon*) of Tyne & Wear, Joyce and Pete (*Sherry Brandy* and *Cuban Pete*) of Leeds, Stan (*Lightfoot*) of Leicester, Billy (*Ten Speed*) of Liverpool, and a block booking from the Romeo Hotel Club of Radcliffe, Manchester. These were Pete (*Buck Rogers*), Steven (*Beaver*), Doug and Sharon (*Mr. Pastry* and *Shazbat*), Don (*Sea Pidgeon*) and Frank (*Chieftain*) who can all be reached via PO box 2, Radcliffe, Manchester. Some, and I do mean some, of the clubs I spotted at these eyeballs included Blue Star, Formula One, Trailblazers, Friend Link, Sierra Charlie (who have a new president; best of luck to them, and to Sue Rowlands on her "retirement"). Then there was the Firebirds of Brighton. (Don't forget they have a new address), the Currie Card Collector's Club, Currie Card Bank (same address), County Colour Card Collectors Club, Tango Papa's, Romeo Hotel's of course and Kendal and District Breakers Club.

Over the next few months I'll do my best to give you fuller details of some of these clubs I've just mentioned but regretfully I just don't have enough room in any one issue to cover more than a couple. The Blue Star is a fairly new club, being launched officially at the Stanley Eyeball this year and it costs £2, a large SAE with two first class stamps and between 5 and 10 of your own

QSLer Addresses:

Barry (Yorky)	2 Chestnut Grove, Outwood, Radcliffe, Manchester.
Bill & Angela (<i>Steam Pusher</i>)	PO Box 5, Consett, Co. Durham DH8 8NG.
Allen (<i>The Exploited</i>)	PO Box 5, Consett, Co. Durham DH8 8NG.
Steven (<i>Powerslave</i>)	PO Box 5, Consett, Co. Durham DH8 8NG.
Brian (<i>Scot's Cavalier</i>)	PO Box 525, London N20 0UN.
Joyce & Pete (<i>Sherry Brandy</i>)	6 Fernbank Drive, Leeds, West Yorkshire LS13 1BY.
Stan (<i>Lightfoot</i>)	68 Southfields Drive, Leicester.
Billy (<i>Ten Speed</i>)	LF19, PO Box 8, Liverpool L26 9XX.
Romeo Hotel Members	PO Box 2, Radcliffe, Manchester M26 9DD.

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BA12 9BZ.

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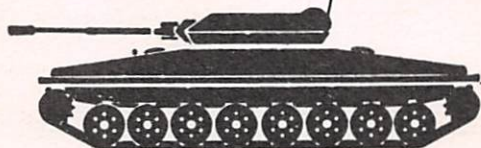
Mode	My Radio <i>ROTEL 240</i>
Channel	Antena <i>SIGMA AVANTI IV</i>
Date <i>15-8-1987</i>	TX PWR <i>6 watts</i>
Time	To <i>DRAGONRIDER ONE</i>
R S	From <i>GORDON</i>

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73's

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QSL



FRANK

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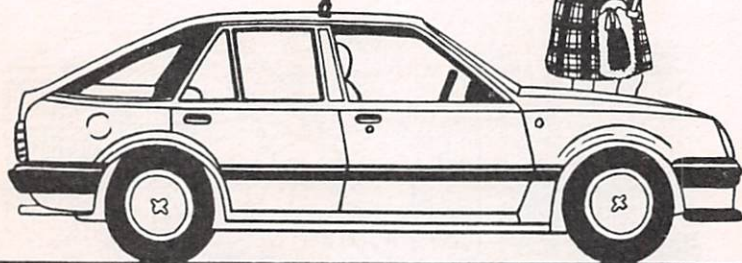
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Forthcoming Events:

July 30th – Alpha Victor Group Eyeball. Contact: AV48, PO Box 5, Ammanford, Dyfed, Wales SA18 3BN.

July 31st – Warminster Eagles Eyeball at Longleat Safari Park. Contact: PO Box 13, Warminster, Wilts BA12 4YZ.

August 6th – Bristol Eyeball. Contact: SBBC 5, 21 Maynard Road, Hartcliffe, Bristol BS13 0AP.

August 13th – Romeo Hotel Eyeball at the Footballers' Arms, Pilkington Road, Radcliffe Contact: PO Box 2, Radcliffe, Manchester.

August 13th-14th – Trail Blazers 3rd Eyeball at the Old Vic Hotel, Wolverhampton. Contact: PO Box 201, Wolverhampton WV10 9HH.

August 20th – Tripe City 2nd Eyeball held at Barton Village Hall, Lancs. Contact: 217 Queens Road, West Church, Accrington.

August 20th – MDX 32 Group Eyeball, PO Box 32, Trowbridge, Wiltshire.

September 00?? – ORC, PO Box 3, Heanor, Derbyshire.

September 11th – MAD DXers at Lansdown Race Course. Contact: PO Box 12, Kingswood, Bristol BS15 5DT.

September 9th-11th – Sierra Tango 6th Eyeball at Robin Hood Camp, Rhyl. Contact: PO Box 8, Rhyl, North Wales.

September 10th – North Wales Eyeball at Bodelwyddan Community Centre. Contact: Harry, 11 Heol Clwyd, St. Asaph, Clwyd, North Wales LL17 0PB.

October 7th-9th – Solway Pirates Mass Eyeball at Southernness Holiday Village. Contact: Eyeball, PO Box 15, Kirkcudbright DG6 4DH.

personal QSL cards. For this very reasonable amount you can look forward to a package containing your BS No, ID card and certificate, 10 club cards, some of the president's cards, various swap cards and invites, along with a club pen, stickers, and viewcard of the club's area. XYL membership is free with her own unit number, ID card and certificate. UK cheques and POs are accepted, but made out to K. Hedley please, not the club.

That's it once again, just enough room to include a quick Browse through a Club's Package, so this month the Club package I'm browsing through is that of the Kilo Kilo DX Club, Oldham Abled and Disabled Aid Group. This one was founded some four years ago by Ken, and the club's other name of Ken's Kommandos. The club was formed by a group of breakers with the aim of helping housebound and disabled people in the Oldham area by obtaining radio equipment so that they could have contact with the world outside. Since starting, the club has obtained such things as wheelchairs, walking frames, walking sticks and so on. Membership to the club costs £2 per year for which you receive 10 DX certificates, 10 club QSL card, use of the PO Box for a year and a few other items as available. To join, simply send your £2 and a few of your own QSL cards along with a supply of SASEs for the return of any QSLs sent via the PO Box. The membership package you get back for your £2 which, don't forget, goes to a good cause, includes the following items. Ten A4 photocopy DX certificates, 10 club QSL cards, membership numbers list, exchange QSL cards from members, information sheets, DTI CB info, and tourist info leaflets. Club extras include log sheets at £1.20 per book and QSL cards at £4 per 100.

That's it, definitely out of room once more, if you want to get in touch with me, please write direct, *not* via the mag! All the best numbers for now, have a great summer, and I'll see you back here next month with some more details of Forthcoming Events and more names and club info!

Out Of Step

Smart Alec examines the 'single market' concept for CB

I freely admit that I am confused! Ever since I first became interested in radio communications people have been telling me that the ultimate goal is harmonisation with our European partners; a single set of standards which will allow one country's manufacturers to take advantage of another's markets for all kinds of goods, so far as communications are concerned, permit radio users within a variety of bands to carry their rigs across national borders without let or hindrance. As we draw closer to 1992 and the 'single market' concept becomes more of a reality, we see high powered TV adverts in which major industrialists extol the virtues of being able to market a single product throughout the entire European continent. The watchword appears to be 'harmonisation,' yet when I look at the rules governing some existing radio services and the proposals for others not yet in operation I am tempted to wonder whether I have been reading the wrong dictionary.

Take for example, the recently introduced 'common' CEPT 27 MHz CB service. Superficially it looks the same in every country which has adopted it. We all use the same channels, the same power output rating and the same transmission mode, so everything in the garden must be rosy. But wait a minute! Things are never quite that simple, even when only a single government is involved and once you start dealing with a whole bunch of them the number of possible pitfalls takes a quantum leap.

In theory, the CEPT CB recommendations were designed to allow British CB users to take their rigs to Europe, whilst French, German, Belgian, Dutch and a whole host of other visitors to our shores would enjoy similar rights in this country. No longer would we endure the difficulties of trying to work out countries allowed four watts output, which only permitted one, who used 20-odd channels and which others (and there were some) required different power levels in different parts of the band. No more would we be subject to

the whim of some foreign customs officer, trying to explain in a language you don't even understand, that your rig has one more channel than his rules allow and so you cannot take it through. No more would rig manufacturers become schizophrenic from trying to meet the demands of who knows how many different national regulatory authorities and no more would 'grey' importers grow fat on sales of rigs of dubious provenance. We would all be using the same rigs, designed to the same specifications and guaranteed suitable for use on the M1, the RN2, the autobahn or autostrada. Well, that was the theory.

Then came the small print. Written into the new regulations was the right of each CEPT participating nation to require that all rigs sold to its breakers meet with 'type approval,' a form of technical examination to ensure that the rigs met certain standards on things like frequency stability, spurious emissions and other such high sounding impedimenta. Again, no problem, in theory! Only when different countries start imposing differing technical standards do the gremlins start to creep in.



All CB rigs must meet a common standard.

We are already discovering that some rigs already imported as being 'to CEPT standard' do not meet the type approval requirement laid down in this country, despite the fact that they have already been approved for use elsewhere. The failure of some models to meet our technical requirements renders their use, even their possession, illegal to

British operators, whilst the spirit, if not the letter of the CEPT agreement would appear to allow other Europeans to use these same rigs whilst visiting our shores. What is more alarming is that some of these rigs are openly being sold for use in this country, despite the right of the authorities to confiscate them wherever they are found in the possession of British CB operators, however persuasive the salesman might have been and however innocently they might have been purchased.

Don't misunderstand me. I do not intend to criticise the DTI for enforcing strict type approval standards. In fact, I support them, for their insistence that all CB rigs must meet a common standard not only reduces the likelihood of interference to other, more important services, it also offers the user a degree of certainty that the rig he is about to buy is of a reasonable quality. As if to demonstrate the value of type approval, we are already aware of some brands of rig which have failed to gain approval in this country at as many as four separate attempts. On the other side of the coin, we are equally aware of others which have passed with flying colours at their first submission, proving that the standard is not at all unreasonable.

Nor does the fact that some rigs with overseas approval have failed to meet British standards imply either that our regime is too tough, or that any other country's standards are too low. Without finding a technically minded linguist to study each and every individual nation's requirements, all that we can say with certainty is that there are differences. The pity of it is that in complying with one country's standard a manufacturer may even find it impossible to meet a different requirement imposed elsewhere.

The real problem lies not in this or any other individual CEPT member nation but in the international failure to agree common technical requirements and we must ask why, when they have spent so long in overcoming each other's prejudices and prohibitions, the CEPT member states have failed to find a mutually acceptable standard for type approval? Surely in an age when nations can co-operate in the building of multi-million pound fighter planes, rockets and satellites, it is not too much to ask that they get their act together on the question of type approval of such a simple thing as a low powered radio transceiver, so that the original idea of freely crossing European frontiers with a single rig finally becomes an absolute reality?

Or will we be frustrated even then? Can we rely on some faceless Eurocrat ultimately to decide that all French rigs must be green, all British ones blue, German models red and Italian ones candy striped? Will we ever see a truly European CB rig?

Community Radio

— Why So Long ?



As ITV phases out wrestling on the box the delayed arrival of community radio suffers another throw out of the ring

"**W**e've designed a new antenna for community radio," observed one of the enthusiasts the other day. "It's shaped like a question mark and suffers permanent interference from the House of Commons."

He was rather more bitter than the bottles of lemon juice with which the Captain spikes his coke these days. For

the cause of community radio, already resembling the progress of a rust-bucket in a decent typhoon, reportedly suffered another parliamentary torpedo in mid May, as reports of legislation hold-up filtered towards the ever-patient lobbyists. Indeed, "The Times" made this delay a front page story on 20th May ("Broadcasting Bill delay is set-back to new stations") though it was hardly news to those trying to get hard facts on the publishing of the White Paper – due early this year. The shortage is that of time, rather than enthusiasm. As Brits consider a return to the tollpikes of yore, whereby travellers paid to use stretches of privatised road,

there seems to be flagging of determination to liberalise the airwaves. Indeed, more and more top people seem to be saying kind things about community radio, albeit in similar tones that might be used to mention the vegetation diet of the yeti (the abominable snowman, who may, or may not, have applied for a licence to operate a community radio station).

As predicted by no few sages, including the Captain, the changes in broadcasting are coming faster than legislation can handle the arrivals – we may still be chewing the cud on satellite TV long after the signals from Continental DBS services have started

bouncing off our garage roofs. Here is the rub, for whilst the original promises of community radio were cut short on the basis that more time was needed to be given for all-round oversight and licencing procedures, so much more has happened in the last year or so, that community radio legislation must now be part of a somewhat massive Parliamentary Bill – and thus, in all probability, lengthy debates in the House – attempting to wrap up the future of Channel 4 and the ITV franchise (the much-discussed frequency auctions of the 1990s); overnight TV output (should these be operated by new franchise holders, or by existing ITV companies?); a new channel on terrestrial television, e.g. ITV3, plus all kinds of developments for radio, not least the three new national channels that will in all probability take AM frequencies away from the noble and much-loved BBC. Any odd noises in the background may be the groans of Lord Reith, creator of the BBC as we know it, from his celestial resting place.

Community radio has almost taken a back seat in the debates, partly because whilst a useful activity, it is not in the big money league represented by other, national media. In any case, there is increasingly pressure for community television, aiding educational and public service activity in cities and larger

towns. Anyone coming across the diverse activities of community television in the U.S.A. will know that it has added a lot of colour, metaphorically speaking, to people's lives. If a fringe activity at times, community television has generated a lot of interest among amateur and semi-professional makers, and some excellent programmes have resulted. So far in Britain, the community radio lobby has done little more than maintained a watching brief on community television, probably because the securing of community radio has proved to be a chore akin to pushing a grand piano up Mount Everest while Ma is actually playing it. In any event, community radio could be implemented quickly, without massive expenditure.

However, on current projections, 1990 might be the *earliest* date for a legal medium. The so-called "super Bill" on broadcasting will not reach the House of Commons until next year, later rather than sooner, with royal assent given sometimes in the summer or autumn. At this rate, even our hard working Prince of Wales might have to say something about it, for in one sense, the delays on community radio represent a form of political carbuncle, not in any sense that party politics has been involved, but rather in the problems of our creaking parliamentary system. good works and bright ideas often have to wait for

parliamentary time, assuming they get a place at all. And there is never enough time.

Captain Sparx believes that there is going to be a lot of verbal fire and smoke about this matter, much of it coming from nice folks whose natural inclination is to raise their hats to ladies, and be polite to the squire. Community radio is not a fringe activity for oddballs inclined towards that sort of thing, but a medium of genuine possibilities to many groups in our society. In some ways, community radio is becoming a sort of symbol of the degree to which the voice of the ordinary citizen counts in the land. So – watch for the growing number of questions in the House, letters to the press and other manifestations of dissatisfaction with the latest delays. As an American said to me, when I tried to explain the delays surrounding the advent of legal CB in Britain, "You mean all that stuff isn't fantasy?" Come to think of it, the whole aspect of legal community radio has a soap opera aspect; but if Buck Rogers actually reaches the 21st Century one of these days, will he come across community radio in Britain? Or will he read on his three-dimensional computer terminal VDU that further delays have been caused by the government's need to include whistling noises from Mars in their promised broadcasting Bill?

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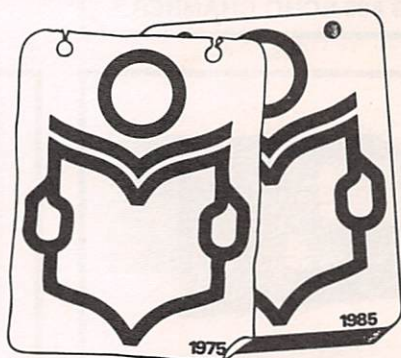
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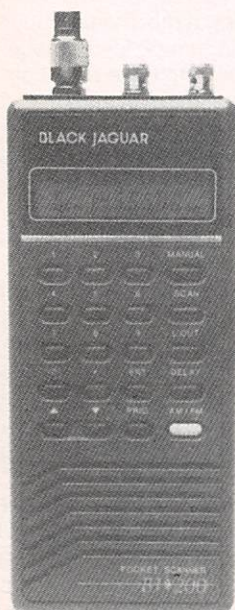
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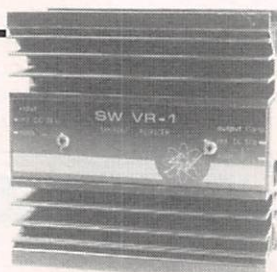
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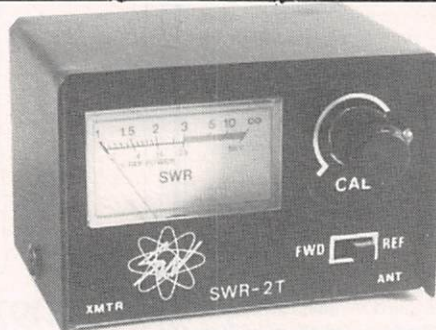
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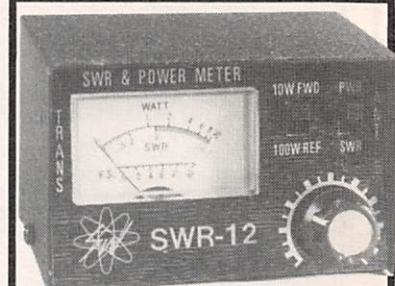
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WHOLESALE

A Cautionary Tale

Linear Lovers beware – we report on a recent court case

Two well known Oxford breakers who have never made any secret of their pride in being loudly heard anywhere within a radius of 35-40 miles of their home, were recently prosecuted by the DTI, had their illegal equipment confiscated and will be paying well over £400 in fines and costs.

Oxford City Magistrates heard how, despite repeated warnings from DTI Inspectors and from their fellow breakers, the couple, Anthony Thomas Swatton, 49, (CB callsign LC Biker) and his common law wife Susan Green (CB call sign Mrs Biker) continued to verbally block legal channel 19 by using unnecessarily strong transmissions from their home near the A40 Oxford Northern By-pass.

Finally, after receiving numerous complaints, the Department of Trade and Industry Radio Regulatory Department monitored their station over a period of time, obtained a search warrant from Oxford Magistrates, and visited their home, together with a police officer. Equipment seized was tested and found to be illegal for CB use. The couple appeared before Oxford City Magistrates on Monday April 25.

The Court Case

Prosecuting for the DTI, solicitor Mr Julian Roche explained that he would be reading from the DTI report, but "for the benefit of the court and indeed myself, I shall try not to give too much technical jargon."

Mr Roche said that after receiving numerous complaints of TV and radio interference the DTI took various readings and registered transmissions at least 25 times more powerful than the

legal four watts coming from Mr Swatton's CB station. Having obtained a search warrant, two DTI Inspectors went, together with a police officer, to Mr Swatton's home, and knocked on the door. There was a slight delay before Ms Green answered the door.

"She asked them to wait, but they pointed out that they had a search warrant and went in," said Mr Roche.

Inside the house they saw Mr Swatton and the DTI Inspector said to him "It's about your burner." (Mr Roche explained to the court that burner is CB jargon for a linear power amplifier).

**"Equipment seized
was tested and
found to be illegal
for CB use . . ."**

Mr Swatton replied, "I sold it last week."

Mr Swatton was interviewed in front of the police officer, then was asked if he agreed that 24 hours before, he had a linear.

Mr Swatton said, "I'm not going to comment now you've cautioned me."

In a back room the other officer found a transceiver. Mr Swatton said, "That's not mine."

Then they found a linear amplifier.

Ms Green was then asked if she had used the CB on Channel 19 at about 5pm that day. She replied, "Yes."

She also agreed that her CB callsign was "Mrs Biker," and that when she was transmitting on channel 19 she was using the linear.



Not Satisfied

The DTI Inspectors were still not satisfied that the linear they had found could have been giving out the kind of power which they had monitored outside, "so they did a more thorough search," said Mr Roche.

Ultimately they found, at the bottom of a laundry basket, an item called a CTE Jumbo Aristocrat Power Amplifier. The officer lifted it out and placed it on top of the freezer while he started to examine it.

It was at this point that the action occurred for which Mr Swatton was accused of obstruction, said Mr Roche.

"Mr Swatton picked the unit up, dropped it onto the floor, saying, 'Now see if it works'."

The officers then told him that they considered that he had obstructed the

"The inspectors took possession of the amplifier . . ."

Warrant and that he would be reported. The Inspectors took possession of the amplifier, noting that the mains transformer was warm, consistent with recent use. Several glass valves had been misplaced due to the amplifier being dropped on the floor.

The items tested included an Electronica 366 radio, which was found to be capable of operating on transmission modes which cannot be covered by the CB licence in this country, the smaller linear amplifier, which produced twelve times the normal four watts of power, and the CTE Jumbo which could produce a *minimum* 25 times increase in power. Such amplifiers used in conjunction with even a normal CB set, can generate harmonics which cause severe interference to radio equipment being used by the emergency services, to private radio mobiles and to TV and broadcast radio transmissions.

Neither of these three items can be properly licensed for CB use in this country.

"Such amplifiers can generate harmonics which cause severe interference to radio equipment"

Mr Swatton's Defence

Chairman of the Magistrates, Mr Dennis



Chiles, then asked Mr Swatton what he had to say.

"Well first I did not realise it was *that* illegal," said Mr Swatton. "The linear amplifier was not actually installed. It was out of line because we had been warned before so it was not in use at that time."

"As far as the obstruction goes, they put the linear on top of another radio, a legal one. I went to take it off there and it accidentally dropped on the floor, I cannot deny that."

Mr Chiles then asked him about the linear amplifiers.

"All that stuff was bought in CB shops," said Mr Swatton.

Mr Chiles: "Why did you need such powerful equipment?"

Mr Swatton: "I did enjoy talking to people over long distances. If I'd known it was *that* illegal, I would not have done it. As for my wife using it, she didn't know what was inside the stuff at all."

Mr Chiles: "Are there not such things as CB clubs? Don't they tell you what is and isn't legal?"

Mr Swatton: "No Sir."

Mr Chiles: "Then why did you, as you say, stop using it?"

Mr Swatton: "You hear people on the CB say that it's 'Not On' to use a linear, so you take it out of line and revert back to your normal four watts."

Mr Chiles: "How long have you had a linear?"

Mr Swatton: "About nine months."

Mr Chiles: "When did you know it was illegal to use a linear?"

Mr Swatton: "I knew myself about two weeks prior to the DTI visit, but I did still

you can buy this sort of equipment for use in amateur radio, where it can be legally used. A linear like that can operate from 26 Megahertz to 30 Megahertz. CB is on 27 Megahertz, so I didn't think it was illegal to use it Sir."

Mr Chiles: "Thank you."

The magistrates then asked Mr

"You hear people on the CB say that it's 'Not On' to use a linear . . ."

Swatton for details of his financial circumstances. After providing these, Mr Swatton asked – "Could you please clarify that I shall get my two legal radios back, together with the microphone, please Sir?"

Mr Roche said that the Department was seeking forfeiture of the items which had been found to be illegal. Two Harrier CB radios had been found to comply with the licence conditions, and these would be returned.

The Verdict

Both defendants were found guilty.

In addition to having all the illegal equipment confiscated Mr Swatton was fined £200 plus £100 costs, for "obstructing lawful examination of wireless equipment."

Ms Green was fined £80 plus £50 costs for "using a transmitter without a licence."

use it early in the morning to talk to people further away. Incidentally, Sir,

Has Big Brother Arrived?

Keith Townsend looks at the implications of the plethora of in-car communications systems



Have you ever stopped to wonder where the current communications boom is leading us? The days of steam radio have quickly given way to instant two-way communication, satellites and even computers that can talk to each other over vast distances. We can now beam television pictures half way round the world, guide aircraft along radio beams and even pinpoint the position of a ship, or even a car, to within one metre, using satellite guidance systems. These are just some of the undoubted benefits of modern technology, but what of the possible disadvantages? Are we, for instance, in danger of losing a good deal of our personal freedom?

It is only a few short years since the sight of a car fitted with an unusual aerial meant either a taxi or the police. Now look around you as you drive through any town or cruise down the motorway and you are likely to find a surprising variation in antennae type. You will probably also be surprised by the sheer number of cars carrying some form of communication equipment. By now, we are all used to seeing the usual array of CB aerals and even the odd amateur set-up but try counting the number of cars fitted with the short stubby aerals which so clearly indicate a radio telephone and I am sure you will be surprised.

There was a time, not so long ago, when the inside of my car was my own domain, where I could think my own thoughts and dream my own dreams. Nowadays, if the rig doesn't get me, the telephone will. No longer am I safe from my wife's sudden desire that I should pick up the shopping on my way home, or make a slight (five miles or more) detour to collect the kids from school. Nor can I now hide from editors, a race apart from humanity, who appear to suffer from the delusion that we poor scribes can sprout wings and instantly materialise at the scene of whatever event they currently consider might make a good story.

Now, as if I were not harassed enough, they tell me I will soon be able to equip my car with yet another new computer communication system, this time designed to tell me how to drive from one place to another. Well, let me tell you, I don't need it! I already have a wife who is a past master at the art of misreading maps and casually informing me that I should have turned left as the junction in question disappears into the background. Why should I pay hundreds of pounds for a machine which can never hope to match her dubious abilities.

Nor does the intrusion into my private little world end there. In this business it is no use relying on your memory (well, not on mine, anyway), so that my car is also equipped with a remote-controlled

dictating machine which enables me to record facts which are, as yet, fresh in my mind, often hours before I could commit them to paper. (This has been something of a mixed blessing, as forgetting to cancel the "record" switch has, on occasion, amazed some poor typist as she discovered my darker thoughts about my fellow motorists). Now, even this small innovation looks like becoming outmoded, as modern computer terminals are often capable of operation from a 13.8 volt power source, enabling me to write on the move (assuming, that is, that I can still find enough free hands to drive the confounded car). The way things are going, it will not be long before they have installed a fax machine, in the hope that I will transmit copy even before the ink is dry. Given modern miniaturisation, I would not be at all surprised if someone found a way to install a complete printing press in the glove box.

Enough of my problems. I must not forget that even those motorists with no communication system of their own are still subject to the electronic era, in the form of the video camera. As a high mileage driver I am all for improved road safety and have often been grateful to the BBC for those snippets of traffic information which come across from time to time. I also have great admiration for the motorway police, who have helped me out of more than one tight spot, but I cannot help but wonder how many pairs of eyes are watching me as I go about my lawful business.

Could it be that Orwell's vision was right and his timing only a few years out?

Another aspect of the boom in mobile communications which has given me occasional food for thought is that of interference. I can live with the occasional interruption of my listening pleasure, as someone overtakes me, mic key depressed (Thinks: Am I doing the same to others?), but I dread to consider the consequences should my electronically-controlled ignition system or fuel pump suddenly cease to function, in response to a stray signal from any of the ever-increasing number of transmitters I am likely to encounter. So far, no problem, but what if . . . ? And when will we reach the point at which our cars become so festooned with electronic goodies that they begin to suffer from their own complexity. I already know of one radio amateur whose car had to be modified because his indicator lights were reduced to half speed by his own VHF transmissions and some of the electronics in my own car (cruise control, speed warning buzzer, even a device to tell me off, should I have the temerity to try getting out with the lights still switched on) are so complex that I now dread the simple breakdowns which, on older models



would have meant a quick string and chewing gun repair job to last until I got home.

Given the speed at which things tend to happen these days I am often tempted to wonder what the practical limits really are. For years we were told that the spectrum was so tightly packed that there was absolutely no room for a CB facility. Nevertheless, somebody managed to find the space and although recent official reports have stated that there is no large pool of unused frequencies, more economical use of existing bands and the reallocation of others, together with more effective use of ultra-high frequencies, has given rise to a host of new services, with yet more expected to follow. Neither does the public have the only, or even the first call on the spectrum. Modern weapons systems rely heavily on radio and other electronic forms of control and as manufacturers vie with each other to meet increasingly complex specifications we can expect to see even greater demands upon the spectrum.

Already various countries armed forces use radios which skip from one frequency to another at an astounding rate in order to avoid detection by a potential enemy and as such rigs become more widely used I have a mental image of them all tripping over each other as they search for unoccupied channels. Within the public domain there is the prospect of a new "personal" radio service, known to many as Short Range Radio. If such a facility is introduced and then gains the same degree of popularity which it has found in such countries as Switzerland and Japan then we can look forward to another small corner of the spectrum becoming somewhat over-populated, as shoppers, hikers and even some small businesses take advantage of the added degree of privacy which the automatic search for a clear channel is likely to afford.

Despite ever-increasing costs, business radio services are expanding just as fast as frequency space can be allocated to them and recent moves

towards spectrum pricing and channel sharing look like being the only way forward.

However well its use is distributed, we cannot escape the fact that the spectrum is a finite commodity and that one day the bottle will be full. So where do we go from here? Sure, the number of CB operators is not what it was in the early part of the decade and, if you live in the right place you might even be lucky enough to find a clear channel occasionally. For the moment we are blessed with two blocks of 40 channels but the Government has already intimated its intention to re-allocate the upper part of the band to some, as yet, unspecified service within the next few years. Nevertheless, it must not be forgotten that what caused the mass interest in CB in the late seventies and early eighties was the ease with which we could contact overseas stations and it would seem reasonable to assume that the loss of this facility has played a significant part in what some prophets of doom have described as the decline of CB.

What they have not perhaps appreciated is the fact that the conditions which prevailed in the heady days of 1981 will soon be with us again, as the sunspot cycle begins the slow climb back to its 11-year peak. As conditions improve, it seems reasonable to assume that at least some of the past interest will be reawakened and that 27MHz will once again become very crowded indeed. Even if the returning skip conditions do not attract more users in this country we will find the band increasingly full of signals in strange tongues, as overseas operators work each other, unable to prevent their signals from reaching our shore. All in all, I predict that things are going to start buzzing over the next couple of years.

This brings me smartly back to my original point. It seems that there is nowhere I can go to escape the influence of the communications revolution. Who am I trying to kid? I wouldn't want to; I depend on it for my living. Even so, if ever I catch hold of that Marconi character. . . .



The Team Spirit

First with the latest — we have an exclusive review of the TEAM TRX-404 CEPT rig. This compact in-car unit is currently awaiting type-approval but should be available shortly for under £80. Saul Wright reports ...

Outwardly the TRX-404 looks fairly unremarkable — at first glance it looks like a half-dozen or so other rigs but look again — the switched marked VSQ, and the Mute indicator show that this one is a little out of the ordinary.

Before we begin, a brief few words on the 404's background. It's a CEPT standard rig and the one you see here is an early production sample; so early, in fact, that it had yet to gain type approval at the time of going to press. However, by the time that you read this it may well have received the official stamp of approval. Just bear in mind that there may be some slight differences between our unit and those you'll see in the shops. However, we've borne this in mind whilst compiling this review and as far as the main facilities and operation are concerned you can safely assume that most, if not all of our comments will apply.

Among the 404's main talking points (apart from VSQ) — more in a moment), is a switchable *roger-beep* that 'pings' when the PTT key is released. On the side of the unit you may see a five-pin DIN socket. This is for a plug-in



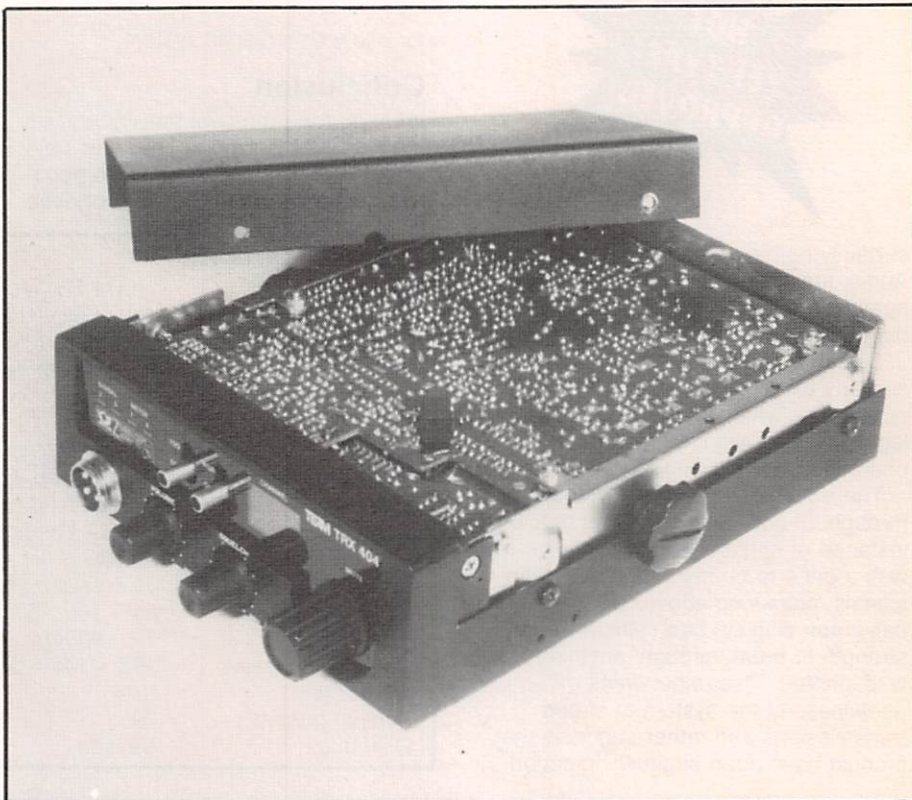
"Any noise will trigger the squelch".

Selective Call (Selcall) module. Now it must be said that at the time of going to press it was unclear whether or not this

facility would be fitted to production units; moreover, the modules themselves may or may not be

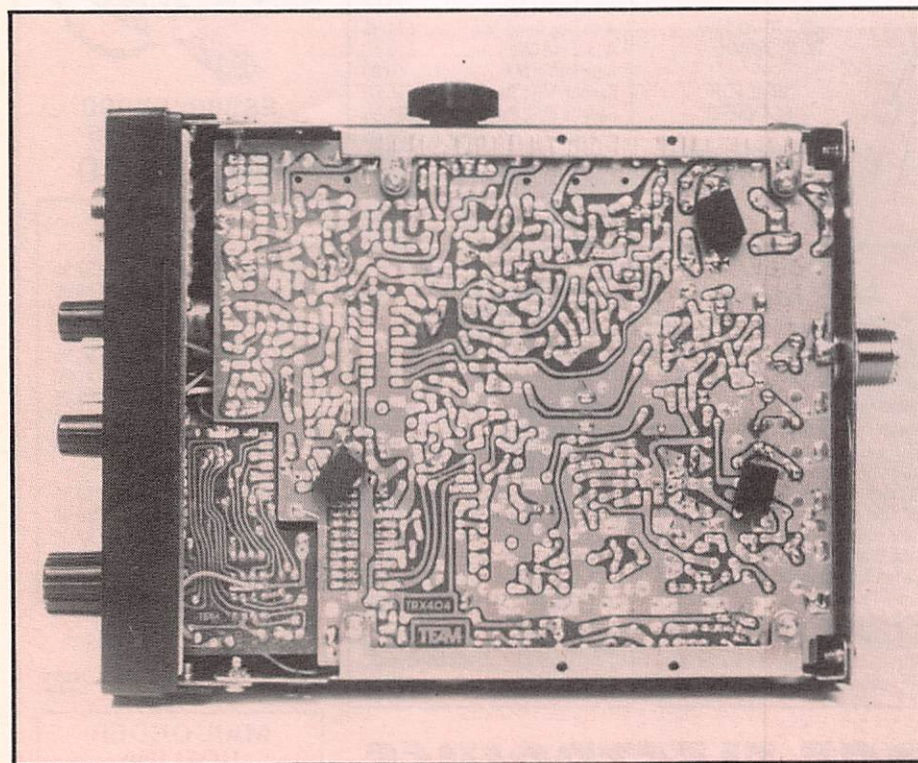
available, so please bear that in mind and check with your dealer first. However, it's clear that the rig is selcall-capable and any reasonably competent rig-doctor should be able to figure the system out if, as we hope, the UK version comes with an English translation of the instruction book, which has a full set of circuit diagrams and PCB layouts.

And so to VSQ; according to the German instruction book supplied with our test sample it stands for *voice squelch*. In fact it's a kind of super-smooth auto-squelch that does away with the harsh 'chopping' of a conventional manual squelch. But why bother? Well, normal squelch works by monitoring the amplitude of incoming signals when it peaks above the pre-set value (determined by the squelch control), it breaks through and is heard on the speaker. Any noise, whether it's a voice, or just a sudden burst of static, will trigger the squelch, providing it's loud enough. As far as we can make out (O-level German notwithstanding) the VSQ system appears to work by detecting whether or not the incoming signal contains AM or FM components, reasoning, quite rightly, that AM modulations will probably be random noise, whereas FM signals represent voice-modulated transmissions, and should be piped through to the speaker.



"Audio quality is crisp".

remains absolutely quiet, most of the time and the only sounds you'll hear —



"The rig is well designed".

The net result is that with the VSQ switch in the 'on' position the rig

in theory at least — will be the voices of other CEPT-equipped CBer's.

There is a normal, manual squelch control, of course and this, like the VSQ facility, is connected to an indicator lamp marked 'mute.' This rather odd little extra simply tells you that the audio output has been 'squelched' (either by the VSQ or the manual control) and that no noise will come from the speaker while it's illuminated. Quite frankly there doesn't seem much point — anyone with a pair of functioning ears (even one will do) will know that anyway. Without wishing to sound flippant, the only people who could possibly get any use from such an indicator would be the deaf, and CB's unlikely to be high on their list of pastimes! The only other reason we can think of is to tell the user there's an incoming signal, should the volume be turned down; but they would probably notice the S-Meter first as it will be flashing away merrily. Strangely there's no Tx light (it should have been where the mute light is) but the four-segment S-meter lights up whenever the PTT key is pressed, so it's no great loss.

The Results

First, general operation — no problems here. At the receiving end, the rig appears to be well designed and accurately aligned. Sensitivity and selectivity are both about average,

**NOT
YET TYPE-
APPROVED**

certainly nothing to complain about. Audio quality is crisp and fairly clean; again, it's not going to win any prizes but otherwise perfectly adequate.

Copies received from the 404 were similarly clean — on test power output, our sample was just a tad under the legal limit and alignment over the 40 channels was well within the specs.

The VSQ system worked well and throughout our tests there was only one instance of noise breakthrough. There was a price to be paid, though — only signals measuring above S-5 (on the bar-graph display) had sufficient strength to break through, anything less was ignored. This rather limits the usefulness of the system to strong transmissions and rather suggests that it could have been originally intended

for use with a selcall system.

Conclusion

Ignoring for a moment the VSQ facility, the TRX-404 is a good solid little unit with few foibles and average to good technical performance. The proposed

price of around £79 is just about okay. Adding VSQ to the equation tips the balance in its favour. It's a genuine improvement over manual squelch, bearing in mind that it will only work properly on strong(ish) signals. Worth considering.

CB Specs/Data

Make/Model:	TEAM TRX-404
Rx Sensitivity:	1uV/1.5kHz dev (20dB S+N/N)
Audio output:	2 watts (8 ohm)
Tx Power Output:	4 watts, FM, 50 ohm
Tx Deviation:	2kHz max
Microphone:	Piezo
Controls:	On/off vol, squelch, rotary channel select, Roger-beep, VSQ (volume squelch)
Displays:	Channel indicator, mute, S-meter bar-graph
Connections:	SO239 antenna socket, ext, speaker jack, DC power
Power supply:	31.2 volts nominal
Consumption:	Rx - 100-150mA (standby) Tx - 900mA
Dimensions (mm):	192 x 145 x 50
Weight (kg):	1.04
Typical price (£):	79
Distributor:	Nevada

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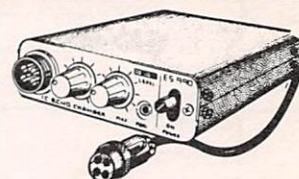
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CAPTAIN SPARX

A SOBERING THOUGHT

**Captain Sparx takes another peek
at US communications**

"Megalomaniacs we have, electronic geniuses clutter the channels every

night — but you don't find drunks on CB radio". The shrewd observation from a CB character in the USA has timely significance for British two-way radio enthusiasts — who might well agree that temperance isn't just a good idea but the only way you can accumulate cash for new equipment.

Drunk drivers are currently as popular as neighbours running a 1000 watt amplifier at the time you want to see a favourite TV soap opera. Even in thirsty Australia, there are genial suggestions that any well-liquored car driver should be given permanent accommodation on Ayers Rock. Though cellular phones represent the fastest growing area of mobile communication in the USA, with sales expected to double this year, from well over \$500 million in 1987, the next big thrust in-car electronics is likely to be the drunk-proof automobile. On present projections, it could become the industry standard by the mid 1990s — and have a profound impact on the UK car business.

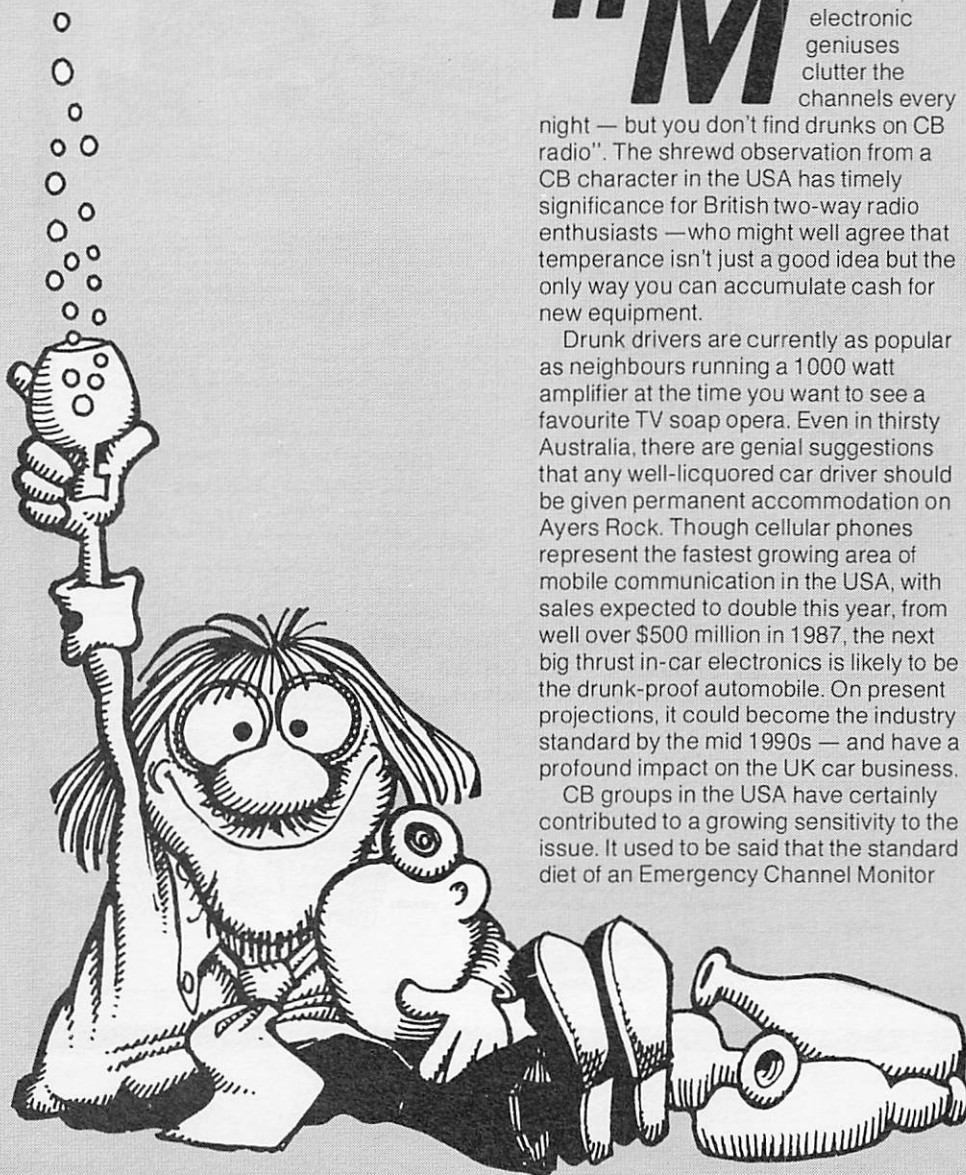
CB groups in the USA have certainly contributed to a growing sensitivity to the issue. It used to be said that the standard diet of an Emergency Channel Monitor

was coffee and doughnuts — a mix that if taken frequently enough, gives one a well-fitted feeling, and an inability to face anything else by way of light refreshment. US monitoring groups over the years have set up coffee and doughnut stands in lay-bys close to major tourist highways, an object being that of reminding drivers about the need for rest during a long journey — and also, of course, about the work on the CB monitoring organisations. Inadvertently, the welcome service also proved important in affirming the supreme importance of driving skills entirely unaffected by alcohol.

"We didn't exactly sing the old temperance choruses", a CB veteran commented a year or two ago. "But when people saw the name of the monitoring group, CLEAR, they assumed we were promoting the use of water". Preferably without all those nitrates that come out of the tap in Britain.

Of course, any modern CB pilgrim making his — or her — progress around the noble CB and hobby radio clubs in Britain would probably get the same impression. Amongst visits recently made by Captain Sparx was one to a pleasant pub on the outskirts of Manchester, where the CB club 'Night Out' had more soft drinks and good sense that you might find, some days, in the House of Commons Bar.

As far as is evident in our official files (an old suitcase full of press cuttings and unanswered mail) no research has been done as to the liquor consumption of CB users, compared to the great guzzling population at large. From encounters with



serious CB users and monitors, the Captain Sparx Research Unit would agree with the old-timer who stated that boozers, on the whole, have no CB awareness (or indeed, awareness of much else on pay-day).

In recent years, CB users in the USA have been part of a growing lobby against drunk drivers. This is hardly surprising; good buddies who became involved in highway and traffic monitoring, and thereby shared views with local police departments soon learned a great deal about the bloody facts behind alluring alcohol advertising. Women also became energetically involved, not merely in publicising 'too low sentencing' but of picketing courts where, to all intent, drunk drivers were treated too leniently. Mothers Against Drunk Drivers (MADD) has been the most influential of these groups — created as it was by a woman whose child was killed by a drunk driver — and is likely to form the pattern for increasing pressures in Britain. In fact, the situation in this country is at least as bad as that prevailing in the USA when MADD and other campaigning groups started in the 1970s.

Ironically, from the early days of Citizens' Band, the boffins have been working towards possible technological answers. From the days of the original traffic monitoring via CB initiatives in Detroit — organised by General Motors Research Labs — two-way short-wave radio was seen as part of a total road safety package. In addition to the driver's thoughtful use of CB radio in one or other of its manifestations, a necessary input would be via in-car electronics, where the vehicle would react automatically to electronic signalling, e.g. from light triggers mounted in posts at the side of the highway, used to control car speeds in hazardous conditions. Ideas included magnets buried in the road surface, activated as required to control car speeds.

How would drivers react, though? Would they not insist upon and use override facilities, ie. to cut out computer control? Maybe — but the CB pioneers thought that public education and humanity's desire for survival would help consumer drivers see the value of electronic performance monitoring. At any rate, local CB clubs in the USA had some highly useful brain-steaming evenings discussing these issues, usually in association with local traffic commissioners, traffic researchers or those ever-lingering CB philosophers. There seems every possibility of a revival of these debates — but with more emphasis on the UK scene where currently some 1,100 people are killed in road accidents every year, and where significantly a growing army of 'victims' (eg. close relatives of casualties) are demanding action.

The idea of an 'anti-alcohol lock' has

long been discussed and is quite feasible. Options include a sequence of operations required to start the car, and designed to baffle — or at the very least deter — the driver-owner well under the influence. Where one is required to concentrate on a choice option, eg. punching in a sequence of numbers on a keypad, this might also help in anti-theft security. There are even suggestions that a dash-board mounted sensor would react to the breath of the owner-driver and cut out ignition when the concentration of alcohol particles on expired breath reaches a danger level. Voice-activated ignition could be a better option since by disconnecting it (as he might the breath sensitive device) the owner-driver might upset his motor more than somewhat. Again, voice activated ignition is advanced as a security measure, as cars could be made to react only to individual voice-prints.

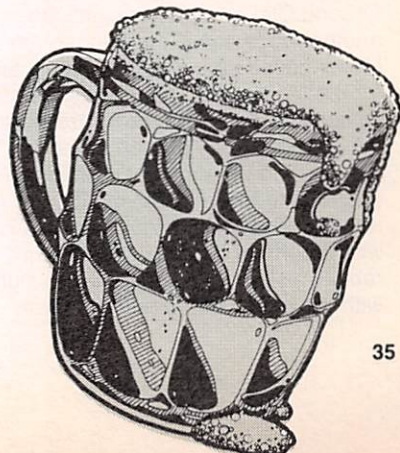
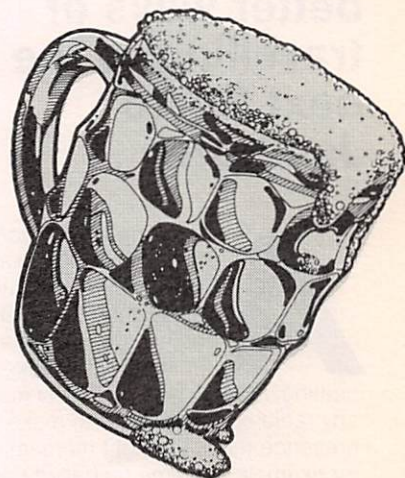
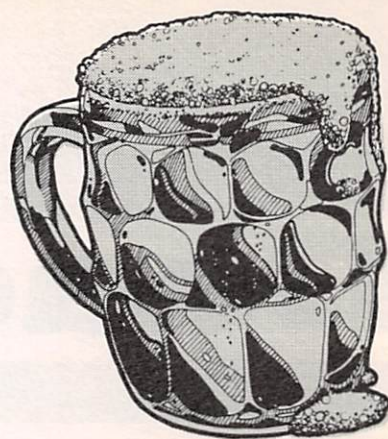
Unfortunately, there is an awesome lethargy about this whole business. The recent mentioning of names of people convicted of drink driving offences, at the end of a recent (November 1987) Scottish Television documentary produced cries of alarm from those wisers and betters who thought such goings-on ungentlemanly. Captain Sparx, who touches nothing stronger than Dandelion and Burdock, and even then only on his cornflakes, thinks that CB clubs and serious hobbyists could help create a more responsible attitude. Having a drink is okay, says he, just as long as you don't intend to put a steering wheel between your mitts.

Can the US CB groups offer an example? Community use of CB and support for campaigns like Neighbourhood Watch, long before the Brits took up the idea — suggest they could. Although cellular phones are becoming even more popular, and offer impressive advantages, there's still plenty of mileage in low-cost CB. And somehow CB seems to have brought people together in clubs, groups, national or local, and in relation to various self-help projects in a remarkable way. Cellular radio seems to emphasise individually, rather than social use, though one cannot generalise.

Whether or not this CB bonhomie could be focussed in Britain, on the drunk driving issue is something else. We may as well leave the last comment to that US CB character whose views opened this soliloquy.

"Drunk drivers have static for a brain and a lunar surface where their liver used to be. Maybe the best idea is to give them a jail sentence, during which they get regular lectures from CB people who know what life is all about".

In case this seems a refined form of torture, I should add that Captain Sparx endorses the idea. There's nothing so likely to sober a fellow up than a long-winded talk about propagation.



Dead Zeppelin

Airships are on the way back, but maybe there are better ways of travelling to the next Radio Jamboree . . .

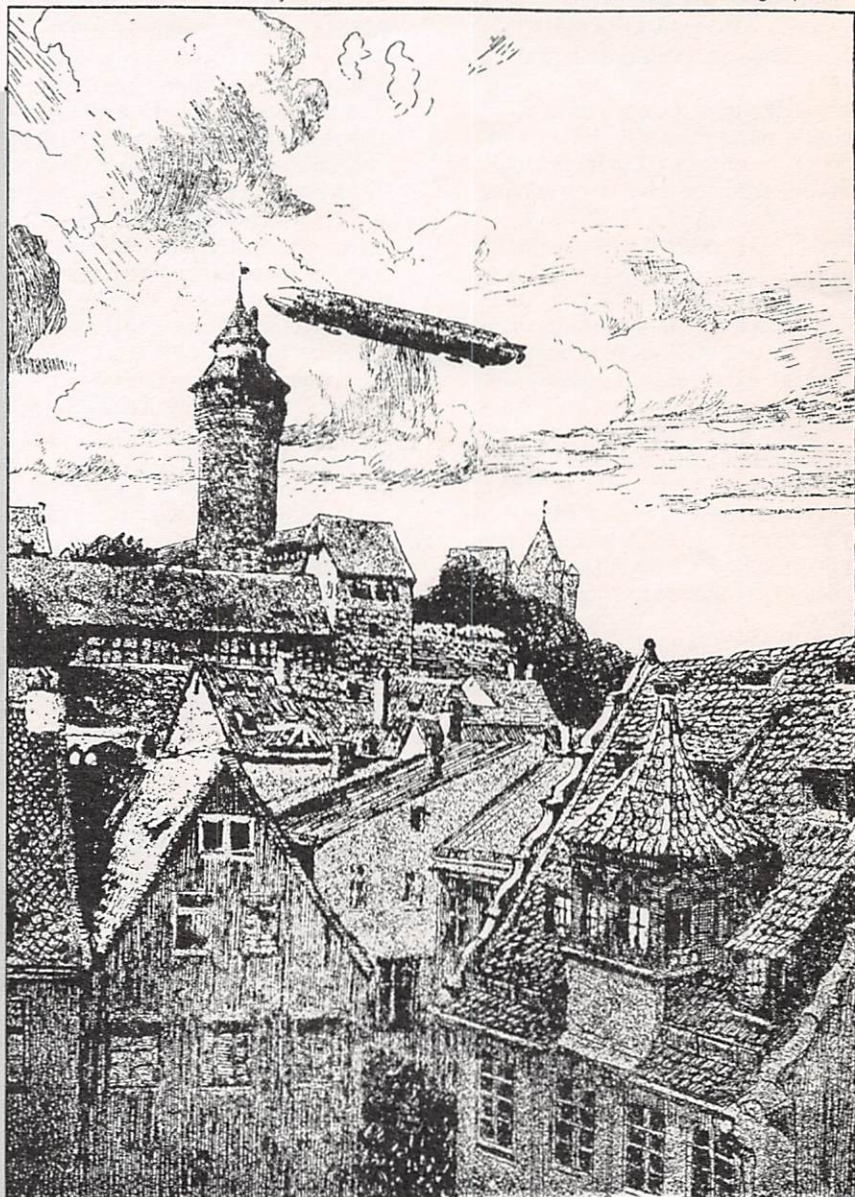
Among the oddest yarns in the Radio Shack, that of Commander von Muller, of the German Imperial Navy, and his spy-ship, the "Emden" would take some beating. Indeed, he is a worthy model for any radio operator eager to make his presence felt at the right moment. Take, for example, the time he called up an Allied ship just out of visual recognition range, with the enquiry: "Have you seen the German cruiser in this area?" The response came in the negative. The Commander added some speed, came in sight of the Allied ship and announced, "There's one about - and I'm it!"

The "Emden" was one of those buccaneering craft that, well disguised, overtook stragglers on the high seas. Indeed, it looked in outline much like a four-funnel cruiser of the Royal Navy, only the fourth funnel was a phoney, a rigged-up charade of canvas and timber. So Commander von Muller hoodwinked Allied ships, occasionally using his radio to add cheering insult. Once, he called the Naval Radio Station at Rangoon, said his ship was available to carry the Allied mails to GHQ, Calcutta. However, the "Emden" proved as elusive as a birthday card posted Second Class on Friday night. On another occasion, Commander von Muller called up a British warship asking for the football results. It is, as they say, "a damn shame" that radio proved to be his undoing. The "Emden" was despatched on a top secret mission to destroy the British radio station on Keeling Island. Everything went well, with a party of fifty German sailors readily dismantling everything in sight - with the speed of a pirate operator

seeing the RIS van park across the street. There was an element of courtesy about all this, for the Commander had excellent manners. Also, the head Brit was able to inform him that a intercepted German radio message had stated that von Muller had been awarded the Iron Cross. Passing the time of day and smashing up radio gear was somewhat interrupted by gunfire from the "Emden", anchored just under a mile off the Island. By the time

von Muller and his merry men had reached their rowing boats, the "Emden" was progressing away at some 20 knots, pursued by a flotilla of the Australian Navy. Subsequently, gunfire from the H.M.A.S. "Sydney" sank the German spy ship. History might suggest that this was an expensive way of keeping a jolly joker off the air-waves.

Germany had the foresight to build a massive radio station near Berlin, at Nauen - forerunner of the high-power



Voice from Zeppelin Gondola: "What do you mean . . . you left the radio in the beer cellar?"



Photographed from a low flying Zeppelin shows Mainan Island, Lake Constance — among such idyllic settings, airship designers planned secret operations.

German stations that so interested hobbyists and amateurs in the inter-war period. It was the Nauhen station that warned German ships on the high seas, of the advent of war in August 1914, enabling them to make for neutral ports. Because Germany had little access to cable services for international communication, there was special interest in military and naval radio. Zeppelins — those monster gas-bags — seemed to have special potential, though radio navigation had its problems. For example, the final significant Zeppelin raid on England, in October 1917, proved to be a disaster with five of the German gas-bags coming down. According to contemporary records, low temperatures at the Zeppelin's cruising height (about twenty thousand feet) froze the generators. This in turn rendered radio communication with base headquarters impossible, leaving the Zeppelin navigators "blind" as well as "deaf". When, against their better judgement, the Zeppelin personnel descended to get their bearings, they were at once picked up by anti-aircraft personnel, in France, with devastating results. The "Zepps" tried diverse schemes to back-up radio communication and navigation. One was the use of an observation basket, lowered by cable from the Zeppelin far overhead, linked by telephone. The single-seater baskets were hardly comfortable, but the observer could help direct bombing as operated by the Zeppelin far above the cloud cover. After the war, one of these cradle clutches recalled skipping above the roofs of Paris, amazed that none of the local gents caught sight of him. In a situation like that, some observers might have asked the way to the nearest toilet.

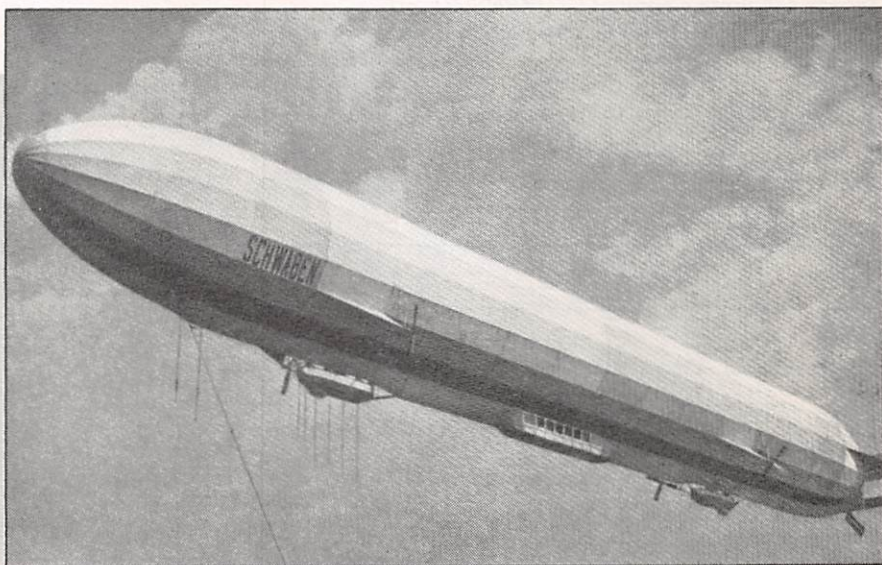
One might say that Count Ferdinand von Zeppelin found his ideas in that home of CB, America. When a mere twenty-five, he went to the USA to serve

as military attaché (from the Court of Wurttemberg) with the Union Army during the American Civil War. During that tour of duty, he travelled in a Union balloon used for observing Confederate positions. Although he went on to shape a military career — in the Franco-Prussian War of 1870, for example, he turned to balloons on his retirement from the army in 1891. He soon found building his Zeppelins prohibitively expensive, and an American millionaire, invited to invest in the project, said he never took any interest in such crazy schemes. The Prussian government was equally enthusiastic, and it was not until 1902 that The Kaiser offered encouragement. Soon afterwards, the Count was given the proceeds of a government lottery (approximately £200,000 today). This is

probably the inspiration for all those CB club raffles we have encountered over the years. Several disasters followed, some due to weather, but Count Zeppelin pressed on. He was interested in using his massive gas-bags for international exploration, including the polar regions.

Zeppelin's airship works at Friedrichshafen, and the airship station at Fuhlsbüttel, near Hamburg, were among the modern wonders of the world. By 1913, a new model, the L.11 had been launched, with a cubic capacity of 27,000 cubic metres, with thrust from four externally mounted motors. The Count, born in 1838, had achieved his 75th birthday, but for all the achievements, the Zeppelins had very limited radio communication potential. There was a shortage of money, a situation known to many entrepreneurs, and this might have contributed to the problem. Had the Zeppelins possessed better and weather-proof generators and radio communication, their effect in bombing would have been much greater.

The post-war record of Zeppelins — and indeed of civil transportation by gas-bag generally — ended in disaster. Today, there is something of a renaissance. Given the possibilities of a low-cost airship for radio operation, there could be a CB Dirigible sooner or later. RID gents will rent the descending observation basket for apprehending those pirate operators using illicit rigs on the top of high rise flats. And quite possibly a distant relation of von Muller will come onto Channel 22, and ask if anyone would like a game of polo on the top surface. My advice: take a parachute, and loose change for the raffle tickets.



Ready for action as war loomed in 1914, advanced model Zeppelin 'Schwaben' — yet these terror weapons had unreliable radio communications.

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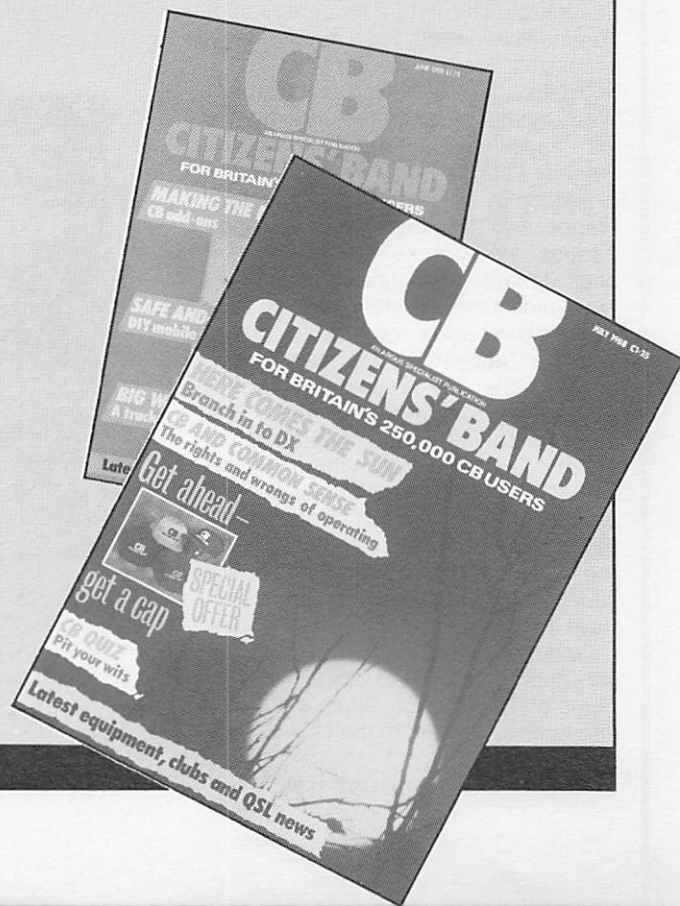
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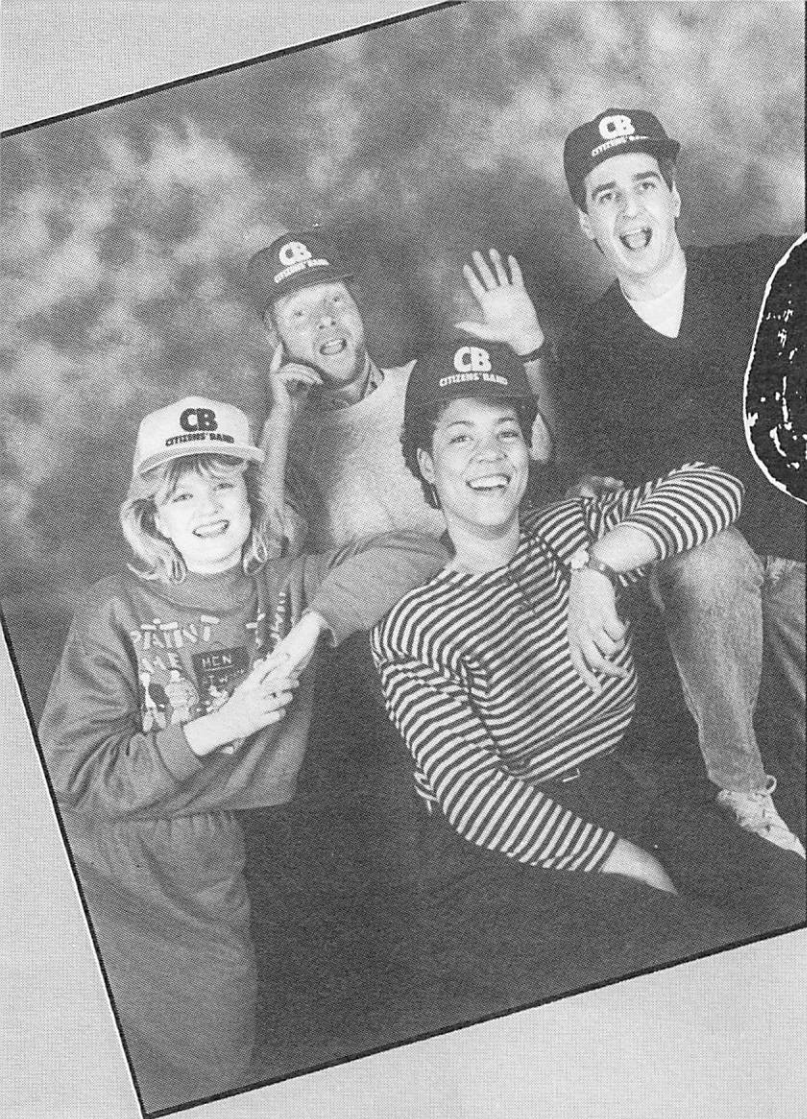
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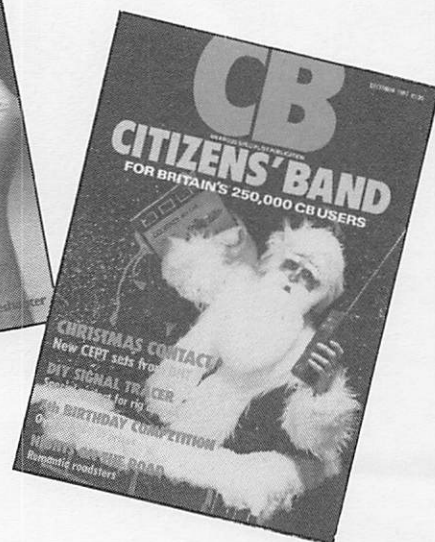
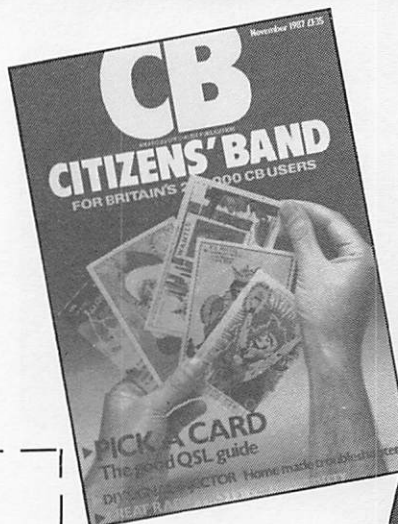
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I made yet another attempt to have a holiday in Cornwall the other week. It was at least the fifth time. As usual, the prospect was deliciously enticing – a cottage on the cliffs, sea, sand, surf, tin mines, Poldark, Jamaica Inn, Cornish pasties . . . holidays in Cornwall should be wonderful. And for the fifth time, I spent the week peering out of a rain-streaked window at grey skies and grey cloud, breaking out in goose pimples at the very thought of venturing out in my brand new bikini, surfboard tucked under one arm.

The reason I'm boring on about my holidays is to set the scene for this month's thrilling instalment from the annals of Little Puddlefoot. You see, cold, damp and disgusted, I decided to come home a day early. I thundered up the motorway – well, inched my way through the contra-flows, crawled past the roadworks, you know how it is – until there at least was the blasted oak. This venerable landmark, which has somehow managed to escape rampaging farmers, hurricanes, even motorway builders, marks the exit which takes you, eventually, to Little Puddlefoot.

But more importantly, it is the point at which I can usually expect to pick up anyone transmitting in the vicinity of Little Puddlefoot. Keen to hear a friendly voice, I switched to the locally preferred channel and waited expectantly. And sure enough, there was Oily Rag.

"She'll get a right shock," he was chortling. "She's got no idea at all – it'll be a bolt from the blue."

Someone was in for a surprise, it seemed. I wondered idly who "she" was.

"It's all rather underhand, I fear," said an earnest, anxious voice. That was the vicar. "These things should really be done out in the open, face to face."

"Come off it, Vicar, she'd never go along with it, not in a million light years. This is the only way, honest. It'll all work out in the end, you'll see. Best for all of us."

Personally, I was with the vicar. Clearly someone was planning to spring a surprise on this "she", and all my sympathies were with her. I've been the unknowing victim of too many "surprises" myself. I wondered what it was this time – surprise birthday party, American-style? I was actually reaching out for the mic to ask them when the vicar said:

"I suppose she is definitely back from holiday tomorrow? You're sure of that?" My hand froze.

Lady Breakers Election Day

Filly returns from a holiday and is greeted with a surprise

"Yeah, yeah, we made good and sure before she left. Back Saturday, she said. I've checked with the milkman and the postman – not a sign, she's definitely off the patch. And Big Jane got a postcard with 'See you Saturday' on it. It's a cert, Vic."

Et tu, Jane, I thought grimly. And was it my imagination or had Oily Rag started to talk like Al Capone? What on earth was this? Was I to be met on my arrival by the vicar, Oily and Jane carrying violin cases?

But no, I was being fanciful., I pulled over into a layby and settled down to enjoy a good old-fashioned eavesdropping session. After all, the airwaves are public, aren't they?

But of course, you know what they say about eavesdroppers – never hear any good of themselves . . .

"I'm sure she'd agree to it, if we only asked," the vicar was saying dubiously. "I know she can be very – well – perverse, but she's a good lassie at heart, I'm sure."

He didn't sound convinced, I thought, scowling at the rig. There was an explosive laugh from Oily which made my fingers crook into claws.

"Perverse? Hah! Stubborn as a mule, more like. Give her the idea you want something and she behaves like the Hound of the Baskervilles guarding a particularly juicy bone . . . no, no, Vic, this is the best way, believe me."

"Well, if you're sure –" said the vicar, cravenly giving in, and before I could weigh in and demand to know what was going on, they both went off the air.

Well, I thought, driving on. Of course, I couldn't be absolutely sure they were talking about me. Maybe there were other "she's" in the village away on holiday, due back the next day, who had sent Big Jane a postcard (wait till I get hold of you, Big J, I growled to myself), but somehow I doubted it. But as for calling me perverse . . . ! Me! Helpful, public-spirited Filly! No, surely someone else . . .

I went around to the pub that night and the usual crowd were there. They not only seemed surprised to see me, they even seemed pleased. I had intended to demand to know what was going on, but somehow, in the face of their (apparently) guileless pleasure at my unexpected appearance, the words withered on my tongue. No, I thought, I'm being paranoid. Forget it. It's probably Maggie from the florist they mean. She's perverse.

I was wrong, of course.

The next evening was the annual general meeting of the Little Puddlefoot and Upper Bumbley Breakers, at which we were due to elect the officers. I was still worrying away at the problem of the "she" who was perverse and in for a shock, and consequently not paying attention. Suddenly, there was a storm of clapping and everyone was looking at me.

"Well done, very well done, very good of you to stand," the vicar was saying in my ear, beaming.

"What?" I said, bewildered.

"You've just been elected Chairman, Filly," said Oily from the platform, and his smile was distinctly malicious.

"We've just voted. It was unanimous!"

I bet it was, I thought indignantly. But without appearing perverse – which of course, I'm not – there was not the slightest thing I could do about it!



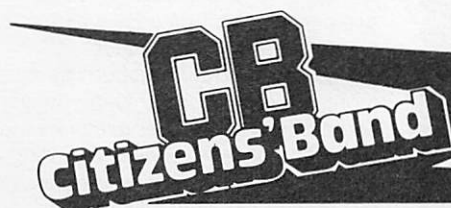
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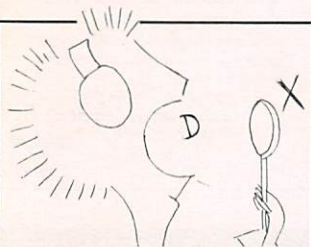
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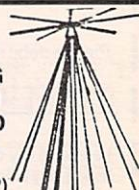
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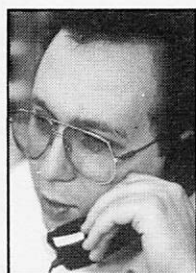
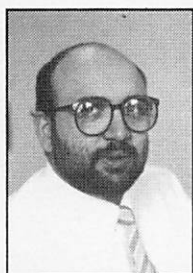
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